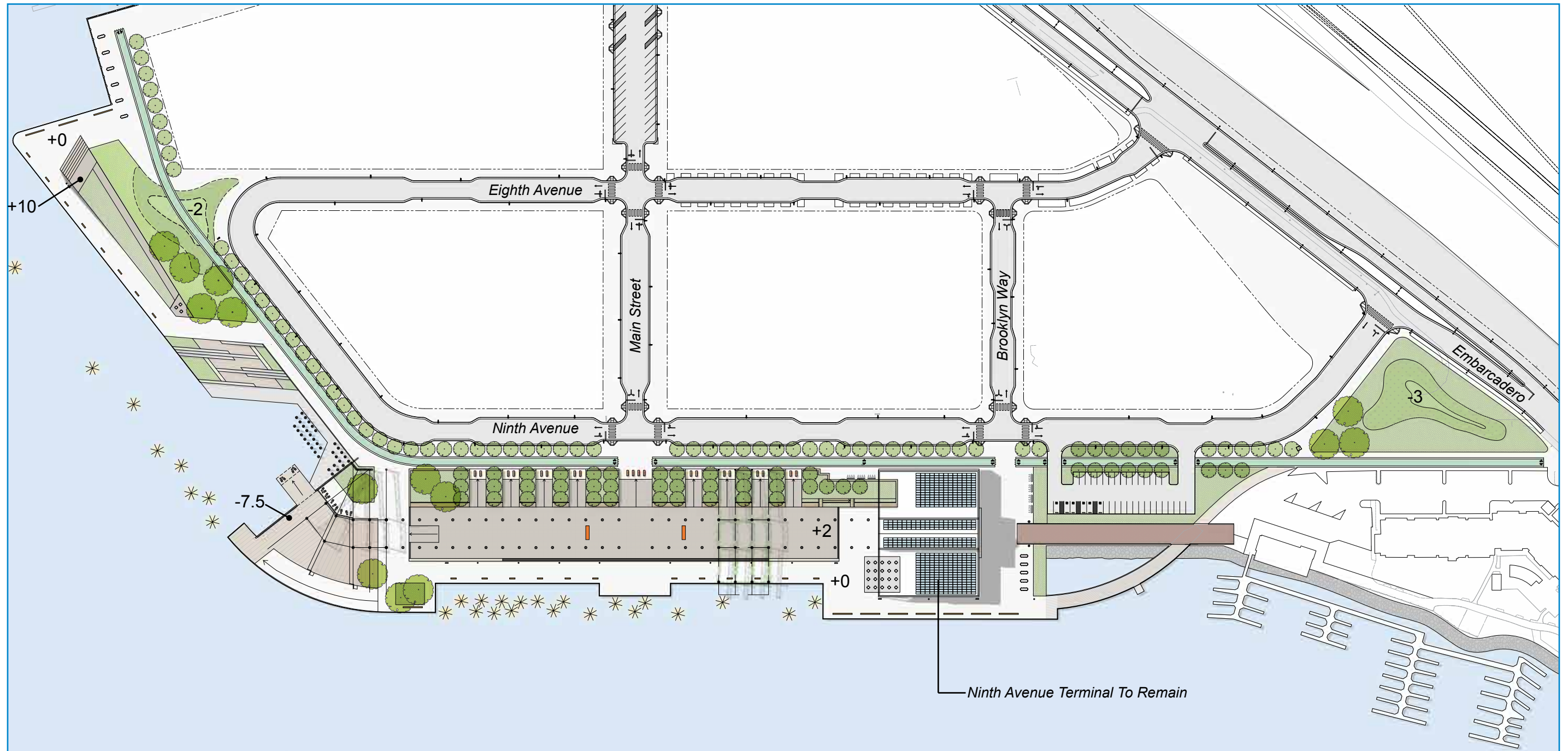




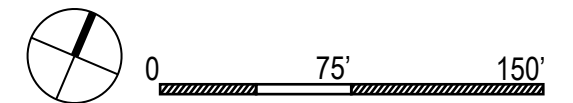
Birds Eye View of Shoreline Park

Number	Sheet List
Sheet	Title
L1.0	Cover
L2.0	Illustrative Site Plan
L3.0	Narrative
L3.1	Narrative
L4.0	Historic Interpretation
L4.1	Historic Interpretation
L5.0	Open Space Context
L6.0	Trails
L7.0	Project Areas
L8.0	Embarcadero Arrival
L8.1	Embarcadero Arrival
L9.0	9th Avenue Wharf
L9.1	9th Avenue Wharf
L9.2	9th Avenue Wharf
L10.0	The Deck
L10.1	The Deck
L10.2	The Deck
L10.3	The Deck
L10.4	The Deck
L11.0	The Cove
L11.1	The Cove
L11.2	The Cove
L11.3	The Cove
L11.4	The Cove
L12.0	The Incline
L12.1	The Incline
L12.2	The Incline
L13.0	Materials East
L13.2	Materials West
L14.0	Planting Wet
L14.1	Planting Dry
L14.2	Planting Trees and Vines
L15.0	Site Furniture East
L15.1	Site Furniture West
L16.0	Public Art
L17.0	Events Plan
	9th Avenue Terminal Architecture
A1.0	Design Approach
A2.0	Existing Photos
A3.0	Design Vocabulary
A4.0	Site Plan
A5.0	View From East
A6.0	View From West
A7.0	North and South Elevations
A8.0	Rear Elevation and Section C
A9.0	Sections A and B
A10.0	Roof Plan



FDP Drawings	Oakland City Datum	MLLW Datum	NAVD Datum
Deck = +2	BFE+55"= 8.20	BFE+55"= +14.08	BFE+55"= +13.85
Existing Wharf = 0	BFE = 3.6	BFE = +9.5	BFE = +9.25
MHW = -9	MHW = -0.06	MHW = +5.82	MHW = +5.59
Near Water = -7.5	MTL= -2.42	MTL= +3.46	MTL= +3.23
MLLW = -14.82	MLLW = -5.88	MLLW = 0	MLLW = -0.23

The concept drawings show elevations relative to the existing finish surface of the wharf. The chart provides the MHW for the FDP drawings and its translation to 3 standard Datums.





Open Space Character for Brooklyn Basin: Continuous and Diverse



9th Avenue Terminal Historic Photo

CONTEXT

Shoreline Park is approximately 9.74 acres stretched along the south and west edges of the Oakland Estuary at the Southern edge of Brooklyn Basin Development in Oakland, California. This park will be the first of four large open spaces within the Brooklyn Basin project, each of which will provide continuous open space built to highlight four very different characters. In compliance with the State Lands Commission, the park will be regional in program and will serve the entire Bay Area.

HISTORY

Approximately half the site was formerly the 9th Avenue terminal. Elements of the terminal building and its setting will be preserved to provide a memory of the industrial history in this location. These historic artifacts will be integrated and overlaid with new plantings, lighting, and other elements that combine with the artifacts to provide a unique infrastructure for a new park. The existing wharf was constructed over a number of years and the condition of the piers is extremely varied and requires upgrade to comply with current seismic codes. The park design considers the wharf condition and develops a new geometry based on repair and removal of existing piers as well as considering the below wharf area as an opportunity for landscape experiences that are closer to the water. Ultimately it is hoped that the water trail will seamlessly integrate with the overall Brooklyn Basin development in more than one location.

ESTUARY

Water and land give way to one another all in the estuary landscape and the park design integrates this watery landscape throughout. Arrival at the park is designed to provide experiences that bridge over water—integrating both C-3 treatment areas, existing trestles, and new near water trails. Effort has been made to showcase the watery landscapes as elements of the park experience. Arriving by from the east at Embarcadero, the visitor passes through a marsh-like planting that may contain some large sculptures that reference imaginary large scale industrial sea life. Arriving from Clinton Basin, the trail crosses a marsh area and provides areas of elevation for better water views.

9TH AVENUE TERMINAL

In compliance with locating BCDC's Bay Trail at the edge of water, a new 30'-0" wide bridge has been added to the east end of the 9th Avenue Terminal to supplement the existing rail trestle—the trestle will remain as a "ruin", but cannot be made ADA compliant.) This bridge will serve as a significant wayfinding element that will direct people towards the 9th Avenue Terminal and provide a dramatic entry to the park. Parking for 38 cars is provided at the east end of the terminal in addition to large amounts of street parking and a new garage about 1 block away. It is imagined that many people will choose to arrive under their own powers by bike, kayak, and on foot and these arrival modalities are given primary location and thoughtful sequencing in the design. Bike parking will be provided at numerous locations within the park. Additional detail on the adaptive re-use of the terminal is provided in the architectural drawings at the end of this set.



Existing View of 9th Avenue Terminal



Aerial: Project Context on the Estuary

THE DECK

At the heart of the park a large south facing deck is located in the footprint of the 9th Avenue Terminal. Oversized ramps evoke boat gang planks and are sited to mark the historic location of the loading dock doors. The terminal's interior trusses are repurposed as trellis armature for lights, plants, art, and possibly heat or shade devices. The west end of the terminal and its geometry are marked by the design of the cove terraces and the rail line that serviced the south edge has been re-inscribed as a large ramp and linear kinetic art—possibly light art in the water if it can be done with appropriate care for aquatic life. A large planter running east west breaks down the scale of the new deck and provides shade as well as large communal benches facing both 9th Avenue and the deck area. Numerous connections to the bike pedestrian path and the retail area north of the park are provided with oversized ramps. A contained space between the truss trellis and the 9th Avenue Terminal provides opportunities for programming inside the 9th Avenue Terminal and on the Deck at a variety of scales.

THE COVE

At the west end the topography of the deck terraces down towards the water in 30 inch trays. These trays reveal the water, the sound of the water, lighting beneath the surface and culminate at the west end of the terminal in a small dock that is floating on the water surface. A curving ramp marks the path of the train and provides universal access to the terraces. From the lowest level some may choose to take a scramble over the rocks for a near water experience to the facing bank constructed of rocks, gravel, and grasses. Remnant piers are left to provide the experience of a ruin and to further explain the wharf structure. The 'official' BCDC trail is provided at the upper level overlooking this cove.

THE INCLINE

As the estuary turns north, long views towards the East Span of the Bay Bridge and the port are dramatically featured at the end of a long grass ramp. The grass ramp covers a public restroom and ends in a stepped wooden stair facing north. Adjacent to this playful incline, a simple meadow with stormwater treatment, trees, and level picnic areas complete the park.

PROGRAM

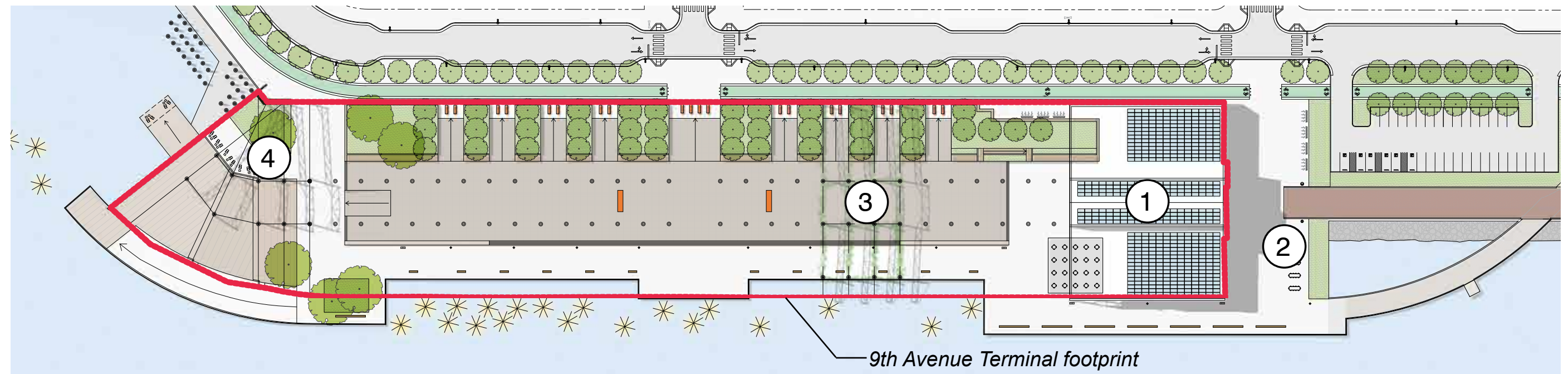
With the re-connection of Lake Merritt to the Estuary, the urban trail network has been strengthened for kayakers, runners, bikers, and many others. Both the Bay Trail and the Water Trail run at the water's edge of Shoreline Park and have the potential to provide wonderful experiences that combine more than one way to travel--bike there, kayak home for example. The development of Shorelink Park and eventually all of the open space at Brooklyn Basin will provide an a fantastic destination on the estuary and continuous open space linking Lake Merritt to the Estuary Open Spaces both north and south. Shoreline Park will function as a tasting menu of what is to come in the Brooklyn Basin open spaces--providing both industrial, formal, urban spaces as well as more natural passive spaces.

The items shown here represent physical artifacts that will be saved from the 9th Avenue Terminal and re-purposed within the park landscape.



1

9th Avenue Terminal
20,000 square feet of the original terminal structure will be saved for adaptive re-use and will feature the eastern facade which can be seen from the freeway and other points east.



4

9th Avenue Terminal Truss

Portions of the light steel truss at the West end of the 9th Avenue Terminal will be re-used for a park trellis armature.



3

9th Avenue Terminal Truss

Portions of the light steel truss on the interior of the 9th Avenue Terminal will be re-used for a park trellis armature.



2

9th Avenue Terminal Setting

The unobstructed view to the 9th Avenue Terminal will be maintained through the use of low or flat interventions that transform the landscape into a park arrival.

The items shown here represent geometries and or locations that will be marked in the park landscape.



2

West End
9th Avenue Terminal

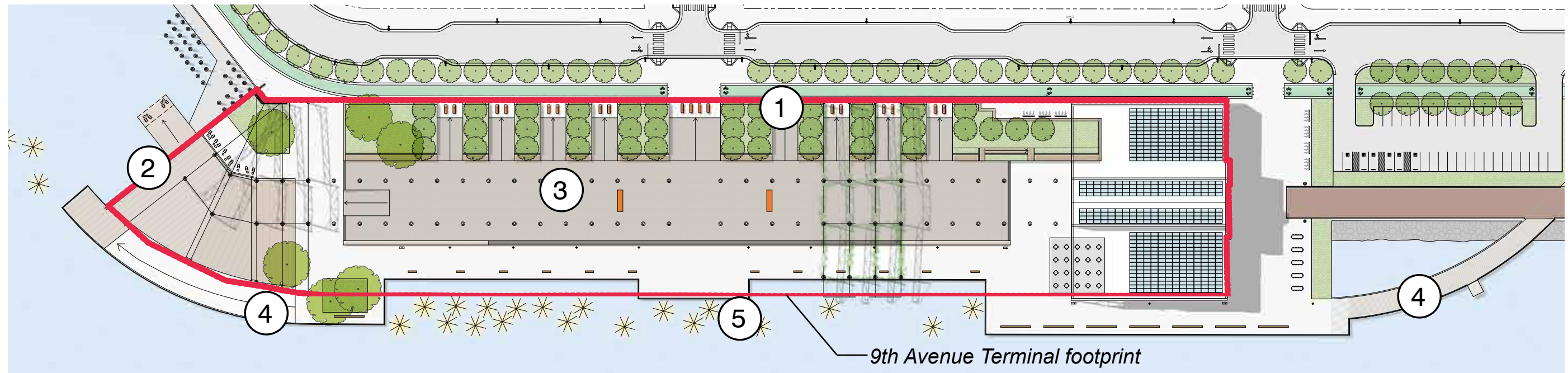
The west facade will be delineated with a seat wall and show the angled geometry and loading door entry.



1

North Loading Docks
9th Avenue Terminal

The loading dock doors will be marked by gang plank style over sized ramps onto the deck--now loading in people instead of goods.



4

Railroad Track Geometry

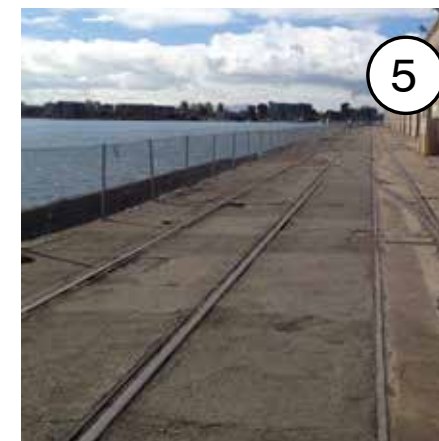
The curving track will be re-made into a long grand ramp down to the water for spectacular universal access.



3

Column Bases

The interior truss column rhythm will be marked in the surface of the deck and the surrounding pavements with steel markers.



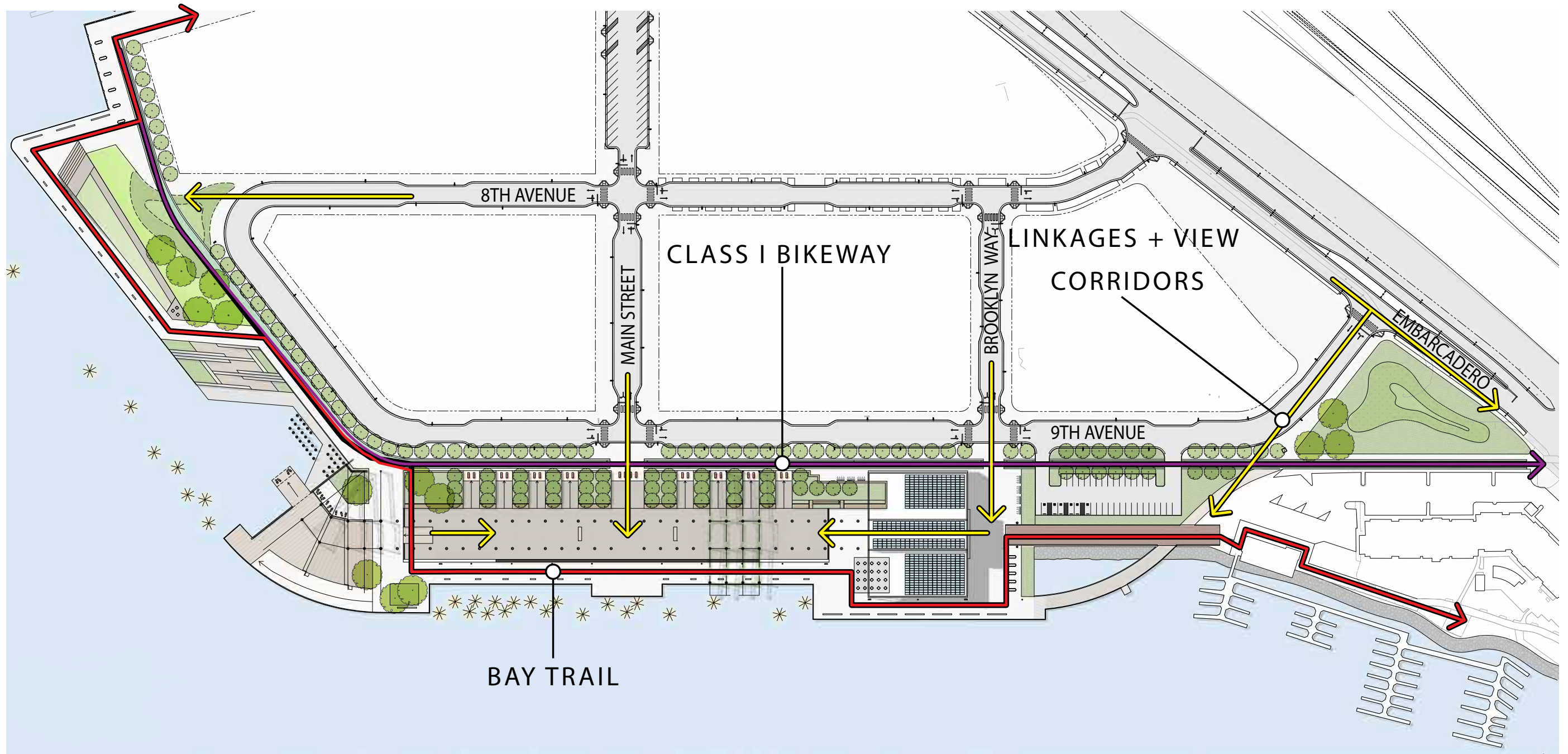
5

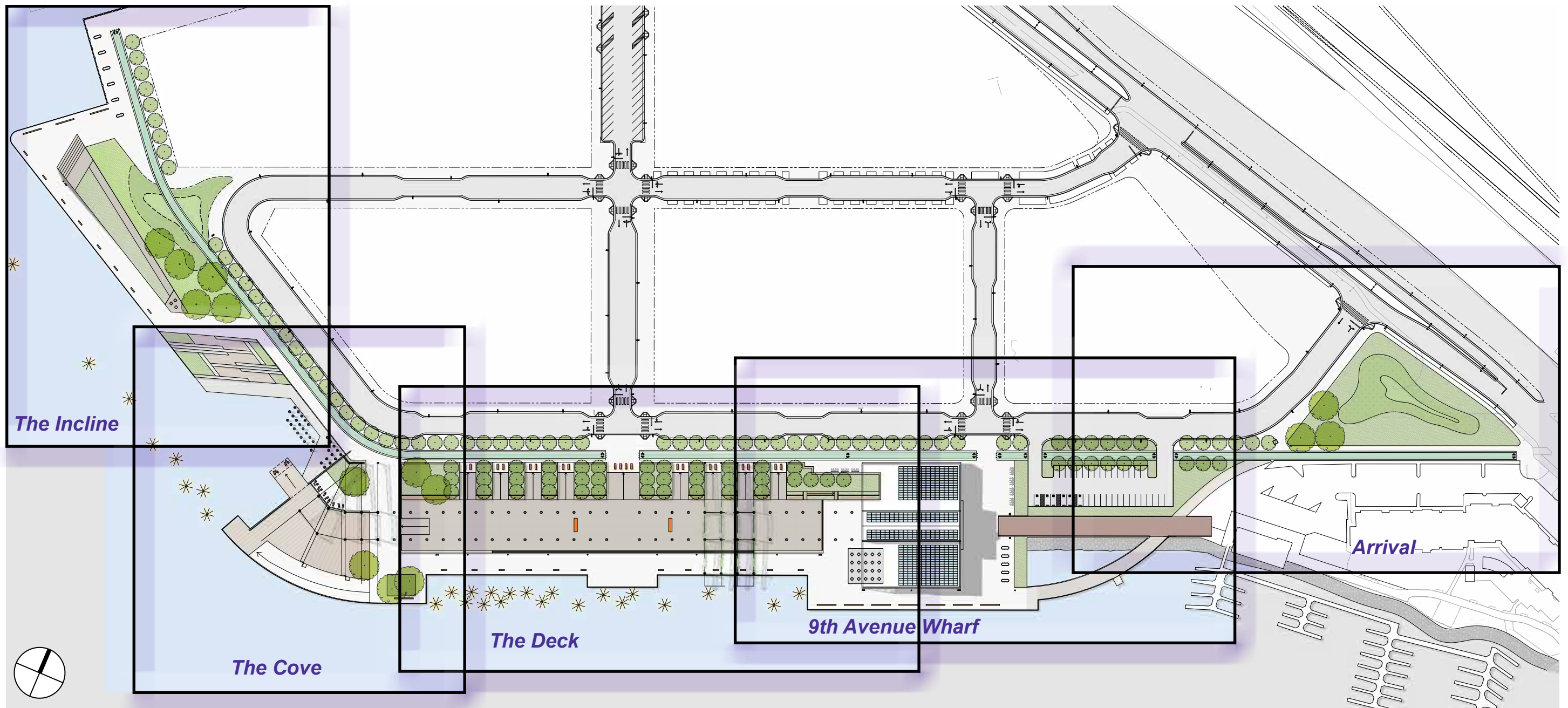
Railroad Track

The railroad tracks will be marked in the water with public kinetic art--perhaps light art that describes movement suggesting the trains as well as aquatic life.

The existing trail network for pedestrians, cyclists, and boaters will be strengthened by destinations throughout Brooklyn Basin. The character of these open spaces will both complement and expand the existing open space along the estuary.







The Incline

Belvedere and overlook of water and long view to Eastern Span of Bay Bridge. Hill for play and drama.

The Cove

Near water experience and reveal of existing wharf structure.

The Deck

Large South facing event space with simple elements to provide adequate shade, structure, and scale. Celebrates history of 9th Avenue Terminal.

9th Avenue Wharf

Visitor Center and Restaurant. Point of entry, historical interpretation, and anchor for large events and programming.

Arrival

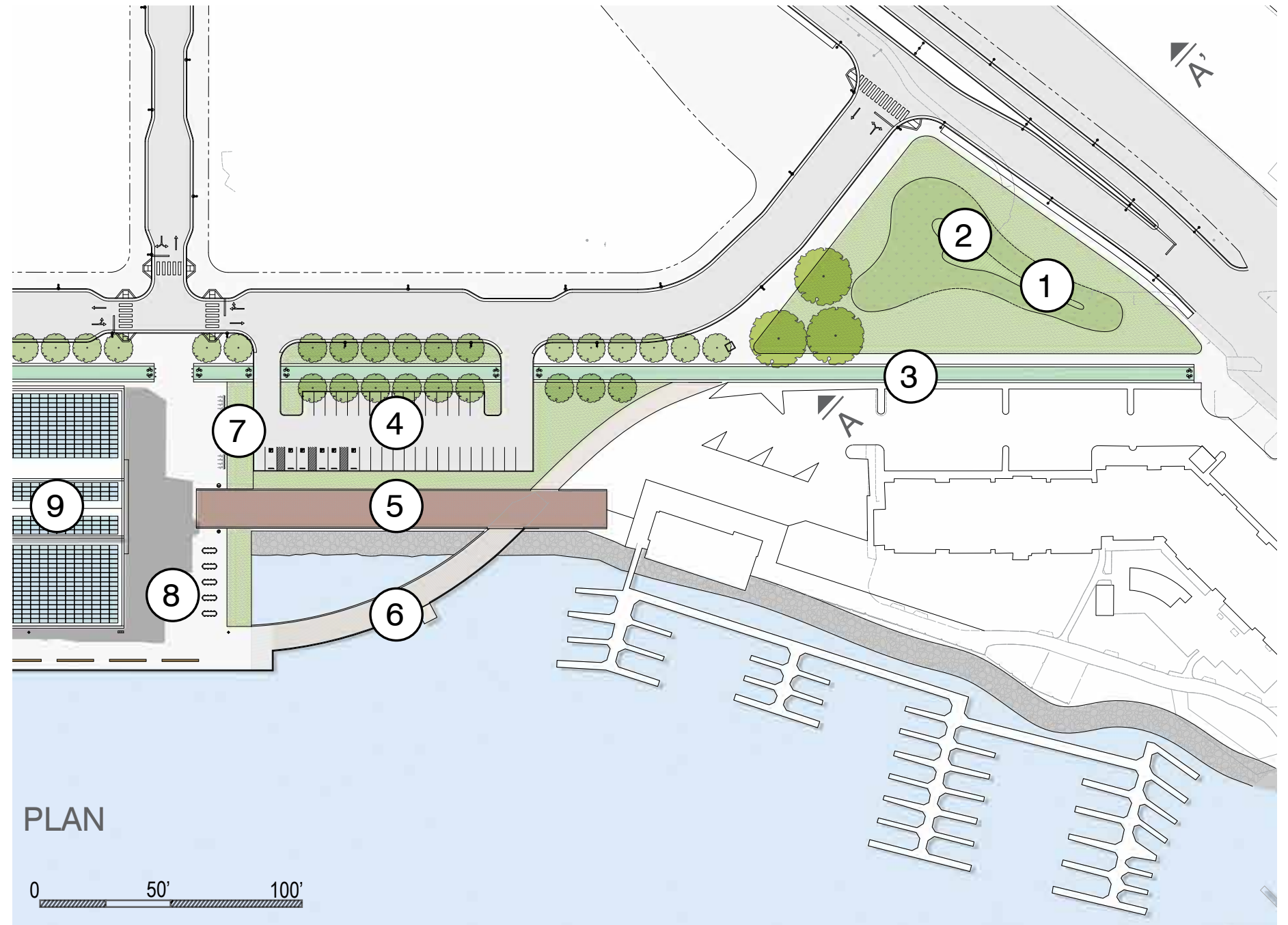
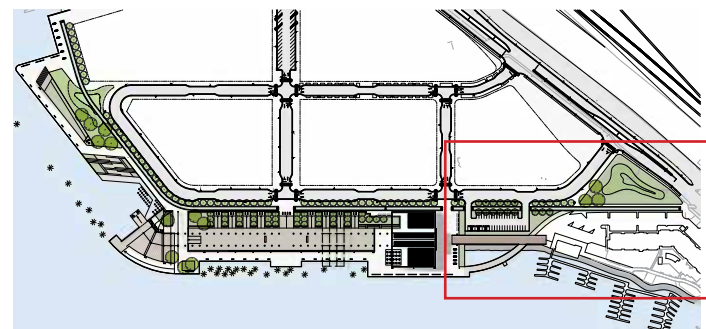
Large scale wayfinding and establishment of estuary landscape experience

Concept Image

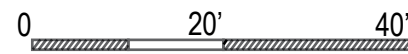
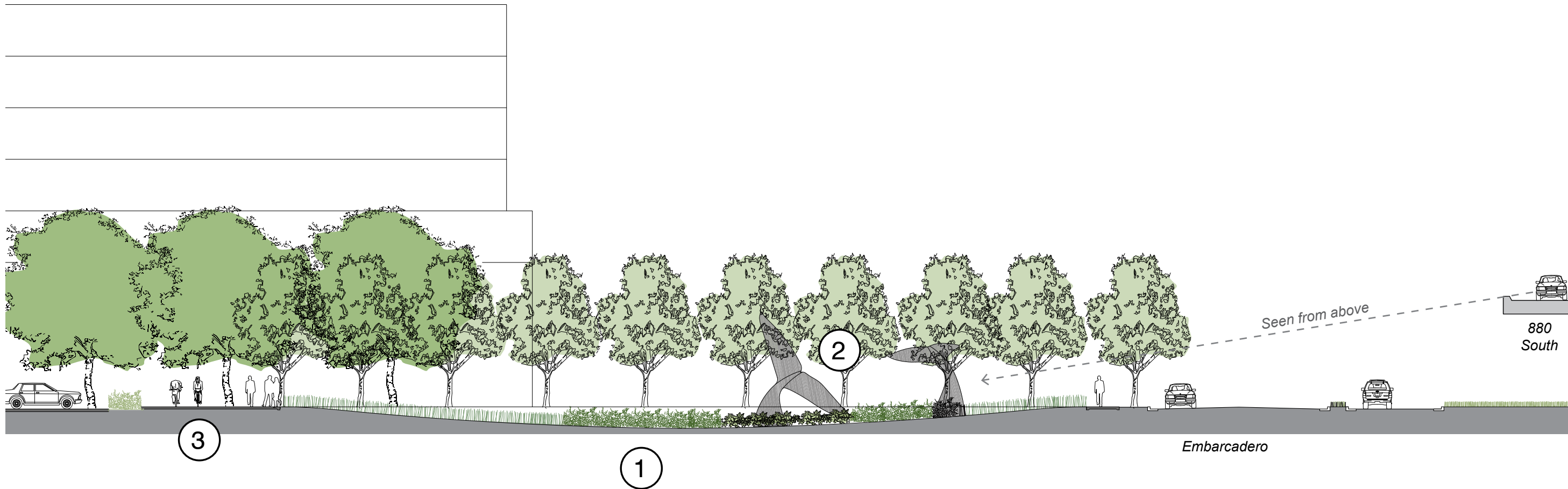


Arrival is designed to provide large scale wayfinding and to establish estuary landscape experience through the use of bridging, plants that grow in watery soils, and public art.

Key Plan



- | | |
|--------------------------------------|---------------------------------|
| 1. Stormwater treatment garden | 6. Trestle walk "ruin" |
| 2. Iconic Public Art | 7. Tall Grasses |
| 3. Class 1 Bike Path | 8. Picnic Tables |
| 4. Parking | 9. 9th Avenue Terminal Building |
| 5. Bridge Arrival (30'-0" Bay Trail) | |



SECTION A



3

Bike Path

A class I bike path will follow the northern edge of the park and link at times with the Bay Trail. It is anticipated that the water trail will also make connections to the Bike Path in the future.



2

Public Art

Iconic large scale public art guides arrival to the park and references the watery landscape of the estuary.



1

Stormwater Treatment Garden

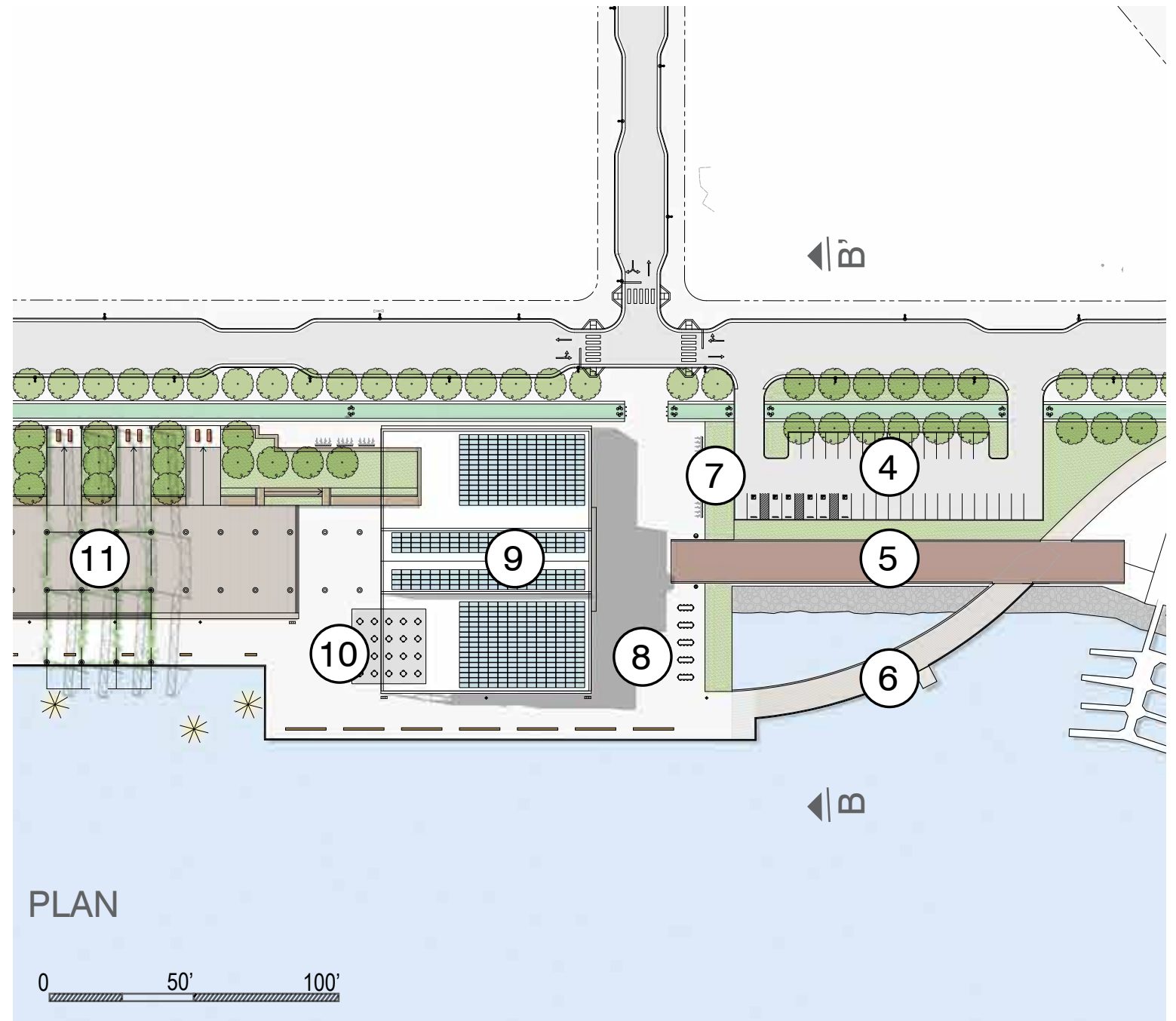
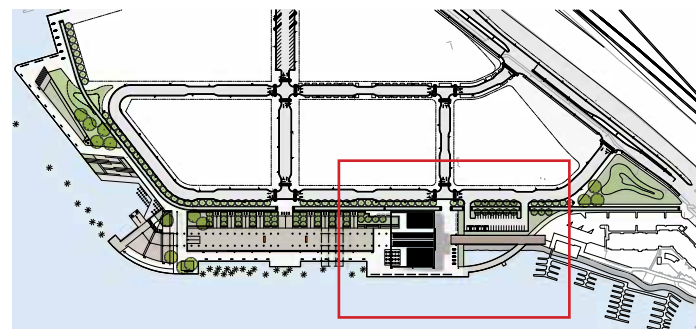
Large scale stormwater treatment is provided and celebrated for its unique plant palette and character.

Concept Image

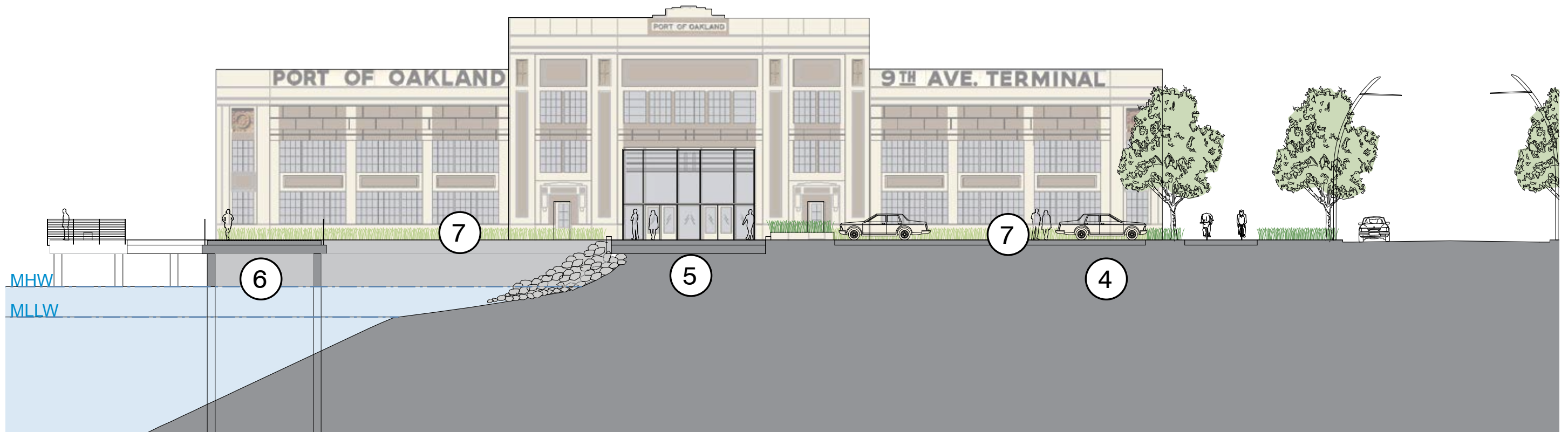


The 9th Avenue Wharf and remaining structure function as a visitor center. A restaurant, historic interpretation, and maps of the site are provided. Significant open space is available for events.

Key Plan

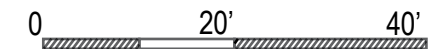


- 4. Parking
- 5. Bridge Arrival (30'-0" Bay Trail)
- 6. Trestle walk "ruin"
- 7. Tall Grasses
- 8. Picnic Tables
- 9. 9th Avenue Terminal Building
- 10. Cafe Seating
- 11. Truss Trellis



SECTION B

MHW = -0.06 MLLW = -5.88
 All grades indicated are City of Oakland datum



7

Tall Grasses in Planters
 Wide plantings of large grasses provide enclosure when seated and blow in waterfront winds.



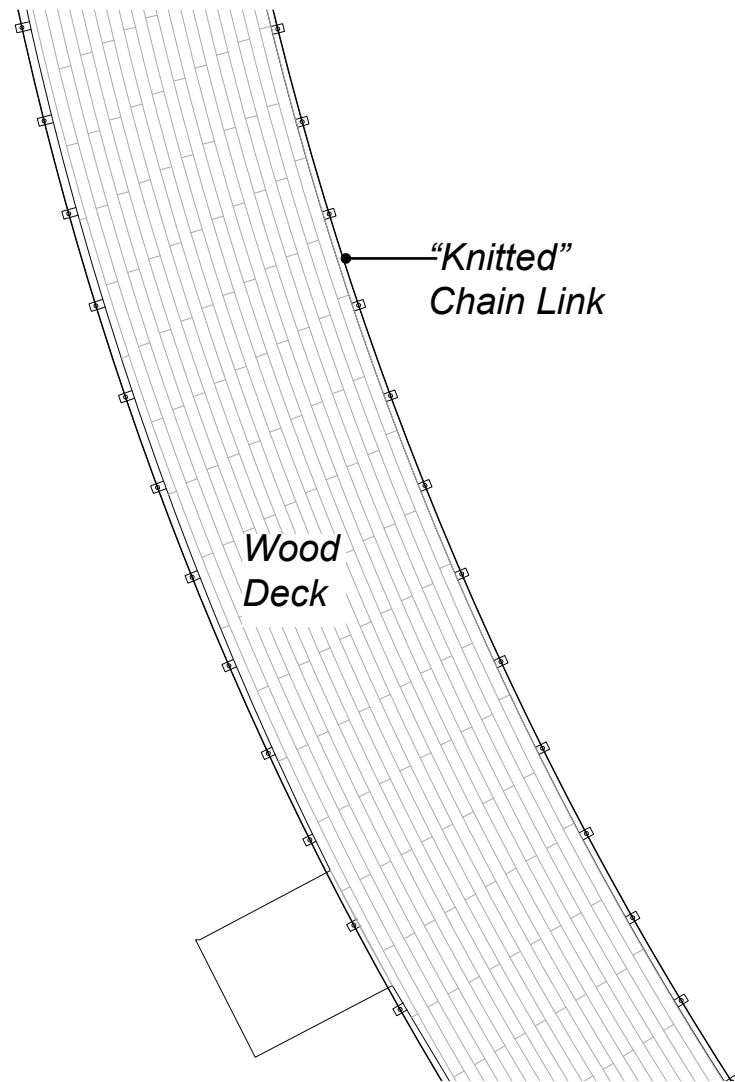
6

Trestle "Ruin"
 The existing wood trestle will be minimally renovated. Guardrail to be re-made with "knitted chainlink" detail, see below on both sides with no infill.

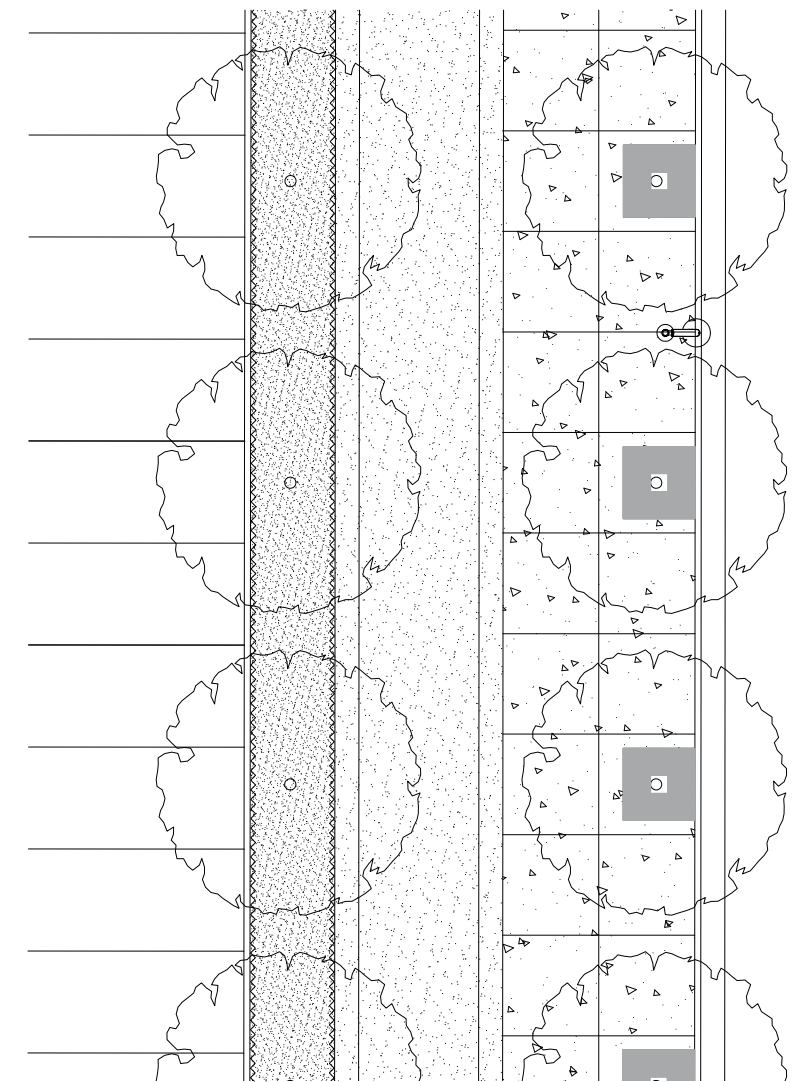
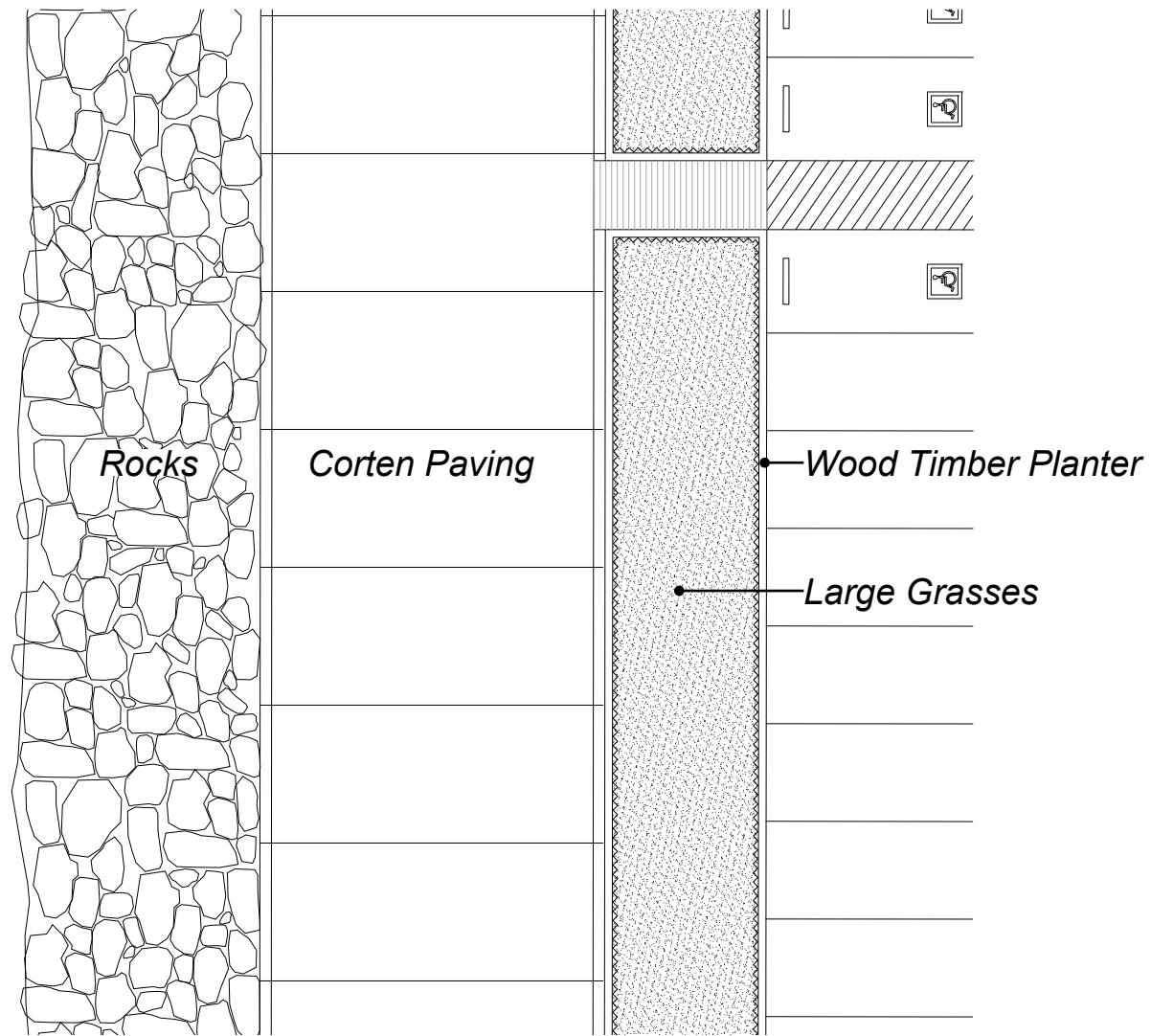


5

Bridge Arrival
 Grand gesture towards 9th Avenue Terminal marks entry into park and introduces the Estuary. Actual bridge will not require handrails--so will be visually flat. See A5.0 for view from bridge.



DETAIL PLAN



0 16' 32'

"Knitted" Chain Link *Wood Deck*

Rocks

Corten Paving

Wood Timber Planter

Large Grasses

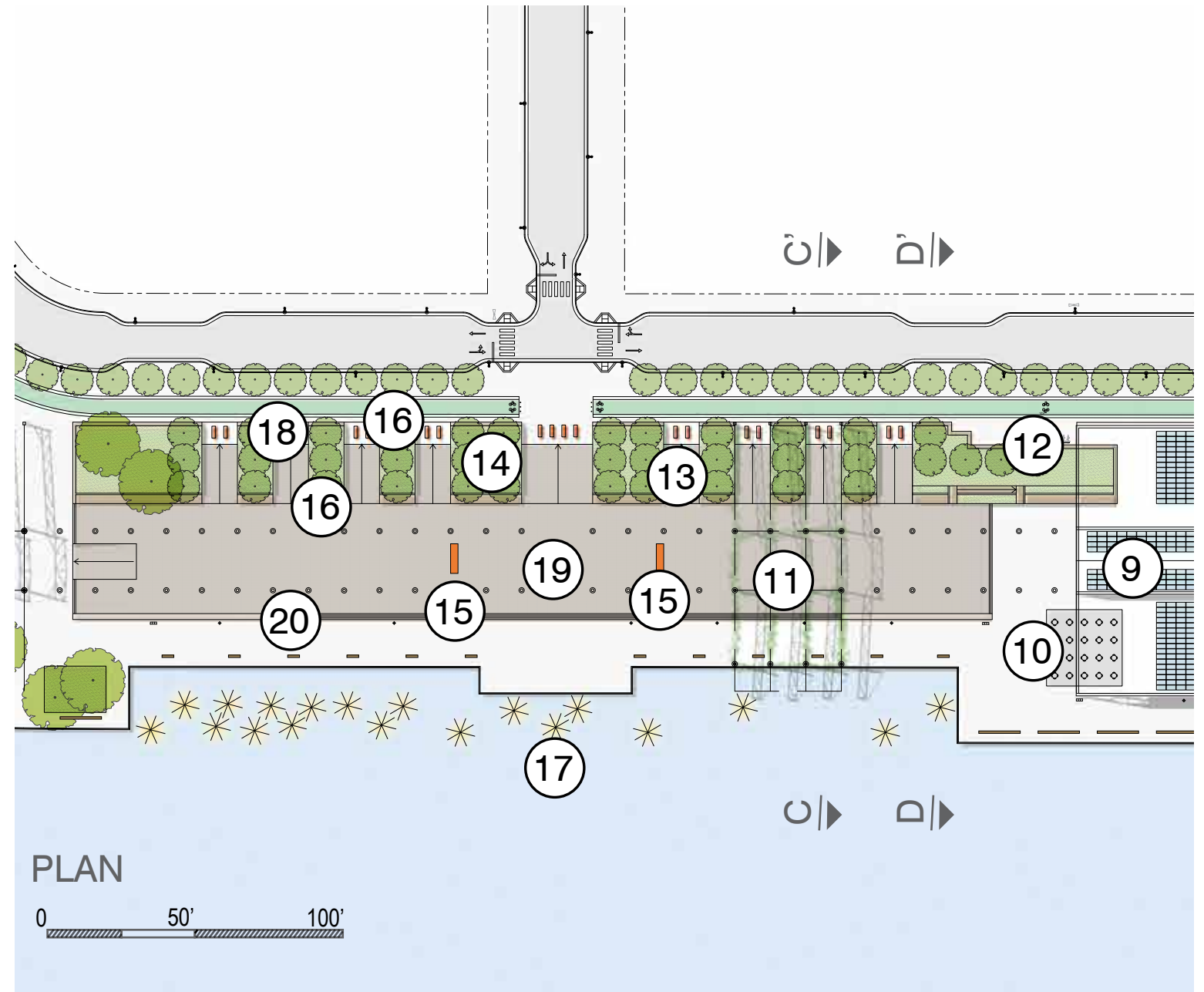
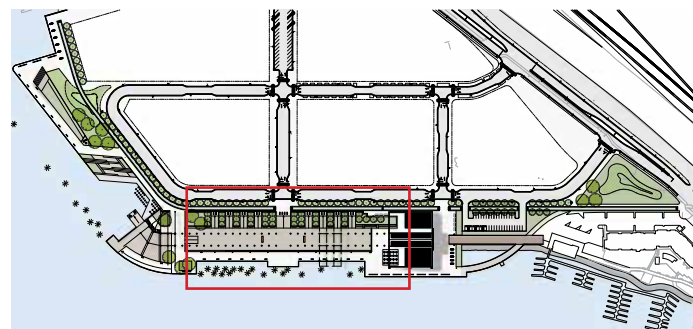


Concept Image



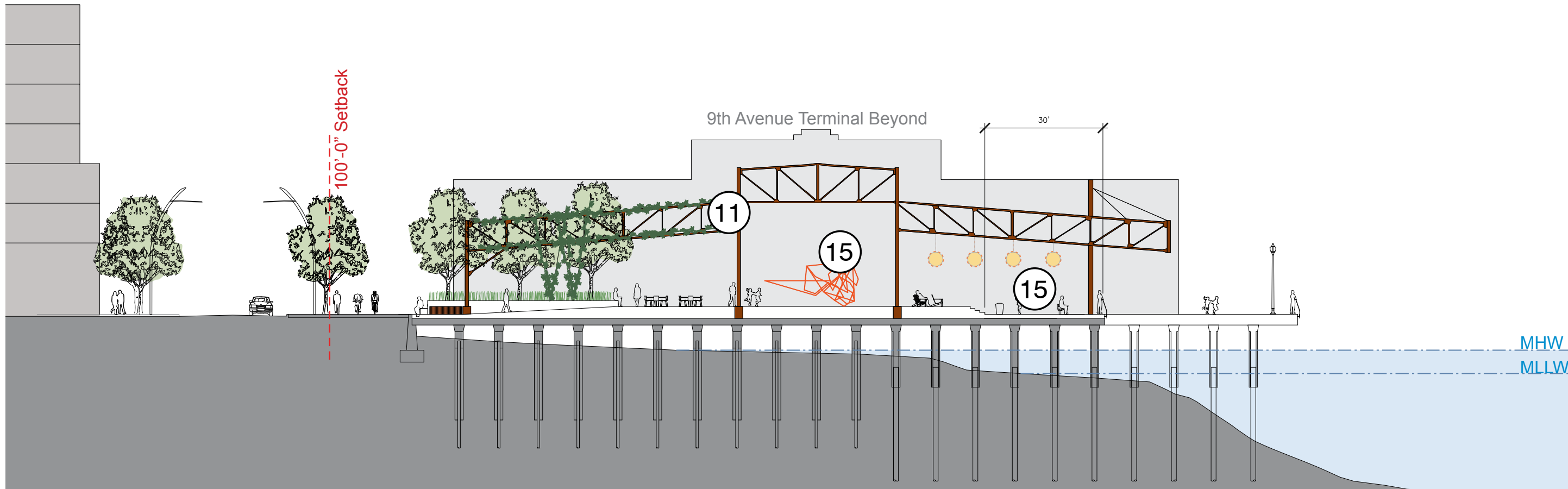
Large south facing deck provides event space with simple infrastructure for shade and scale.

Key Plan

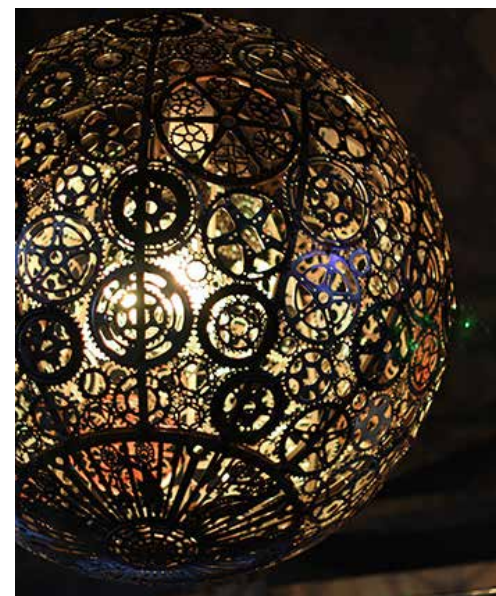
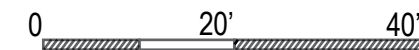


- 9. 9th Avenue Terminal Building
- 10. Cafe Seating
- 11. Truss Trellis
- 12. Bike Parking
- 13. Gang Plank Ramp, TYP
- 14. Large Planter

- 15. Public Art
- 16. Communal Bench
- 17. Linear Light Art
- 18. Metal Bollard Box
- 19. Wooden Deck
- 20. Wood Steps



SECTION C



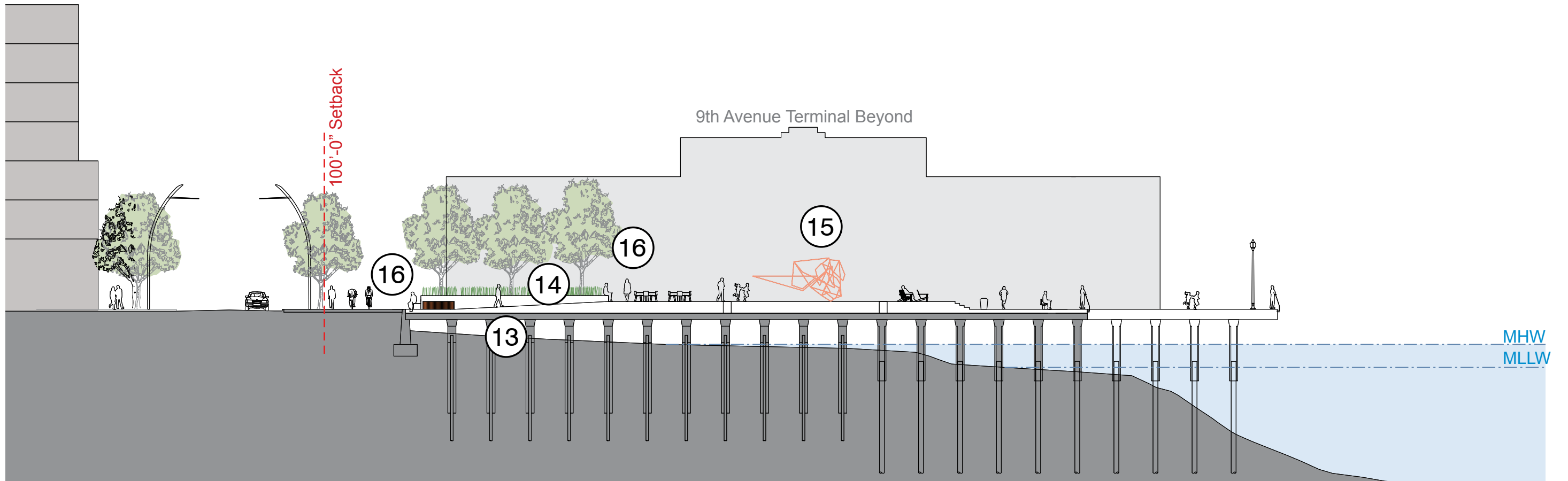
Public Art

Public Art elements that are interactive, provide transformative night lighting, and celebrate the industrial history of the 9th Ave. Terminal will be located on the deck to augment the use and experience.

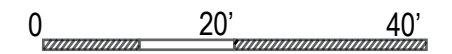


Truss Trellis

Interior Truss from the 9th Avenue Terminal is re-purposed for light, planting, art, and possibly heat and shade.



SECTION D



16

Communal Bench + Large Planter

14

Civic scaled benches face both 9th Avenue and the Deck providing a comfortable edge for waiting and watching as well as plenty of room for enjoying the park.

Large planters form a green backdrop to provide intermediate scale to the buildings beyond, shade, and seasonal change.



Public Art

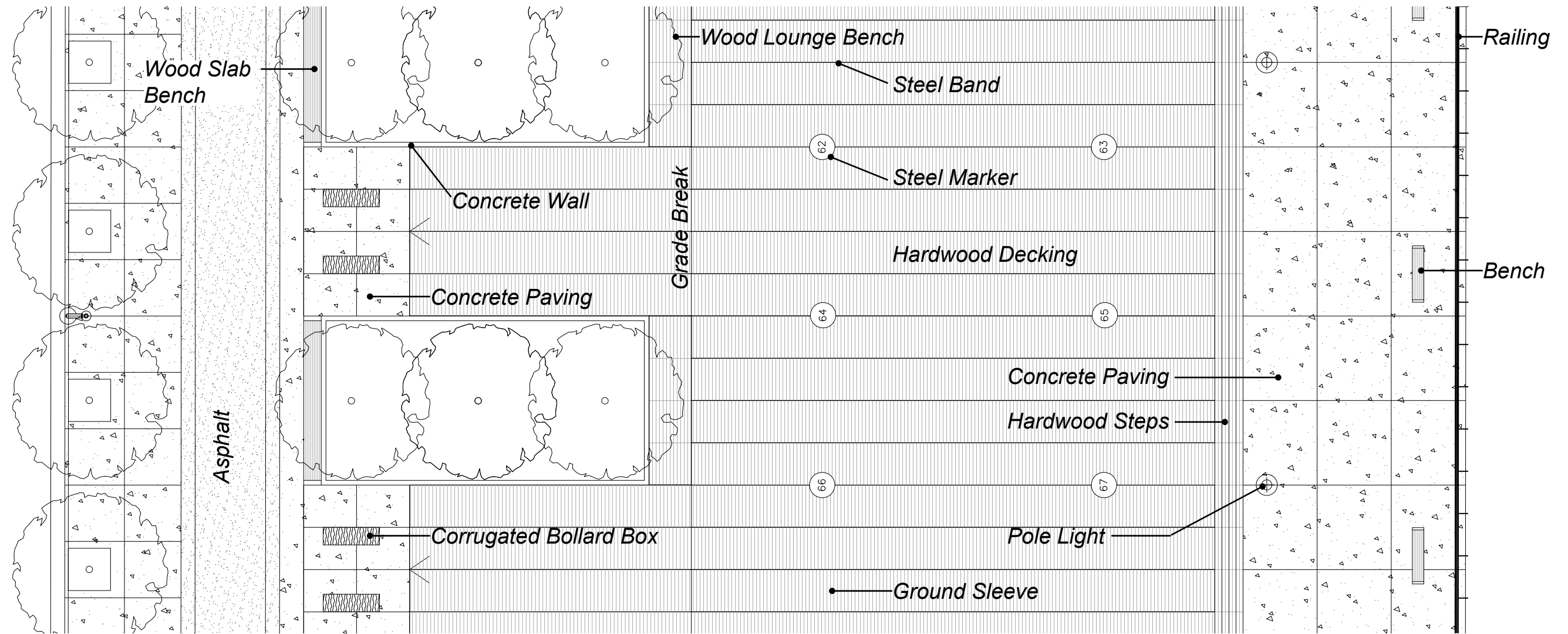
Elements of public art that are both programmed and permanent will populate the deck and provide objects and experiences for playful interaction.



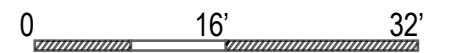
Gangplank Ramps

Oversized wooden ramps provide universal access from 9th Avenue and the Bike path at numerous locations. The decks are hollow below to emphasize the over water experience.

(Image shows concept, refer to section for actual profile.)



DETAIL PLAN



Concrete Paving

Corrugated Box

Wood Slab Bench

Hardwod Decking

Steel Marker

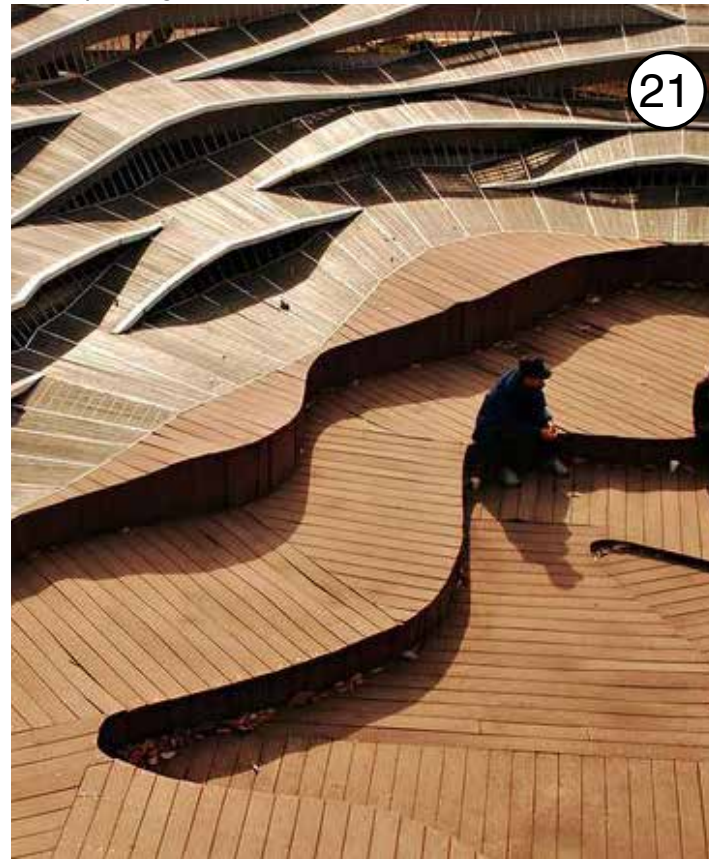
Hardwod Steps





PERSPECTIVE VIEW FROM THE 9TH TERMINAL CAFE

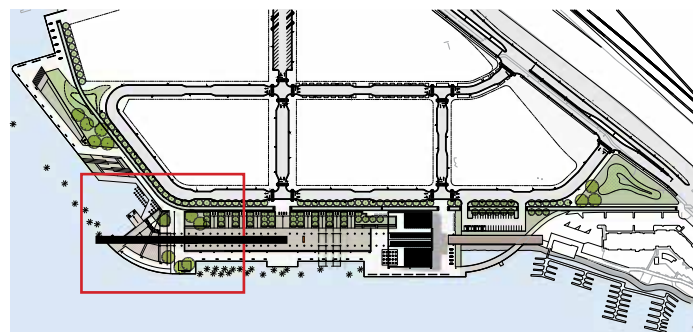
Concept Image



The wharf structure is peeled back to reveal the water below and provide near water experiences through proximity, sound, light, and public art.

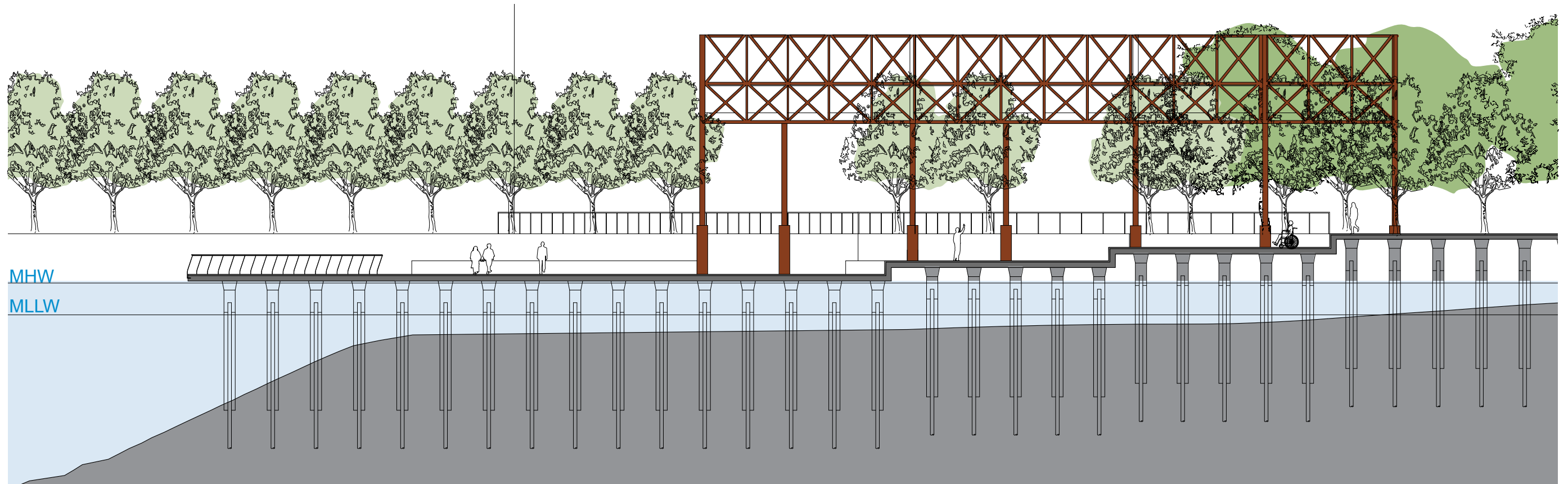


Key Plan

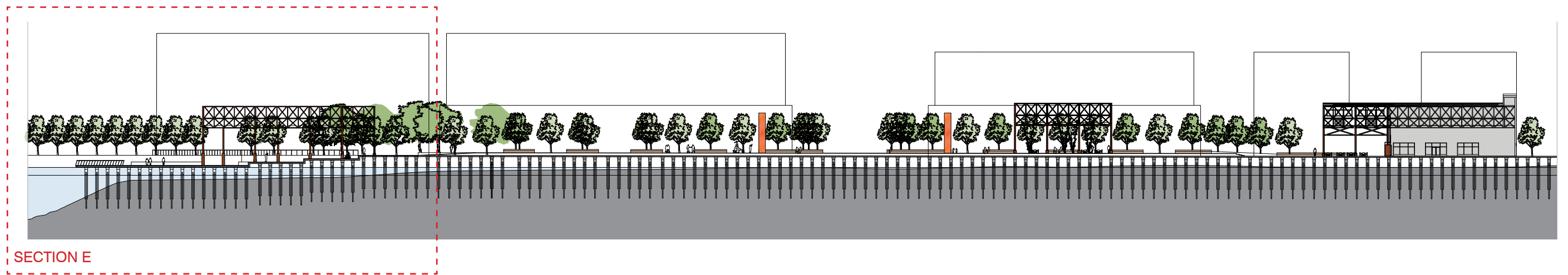


- 20. Wood Steps
- 21. 30 Inch Terraces
- 22. Truss Trellis
- 23. Grand Ramp
- 24. Dock
- 25. Lower 'Near Water' Trail

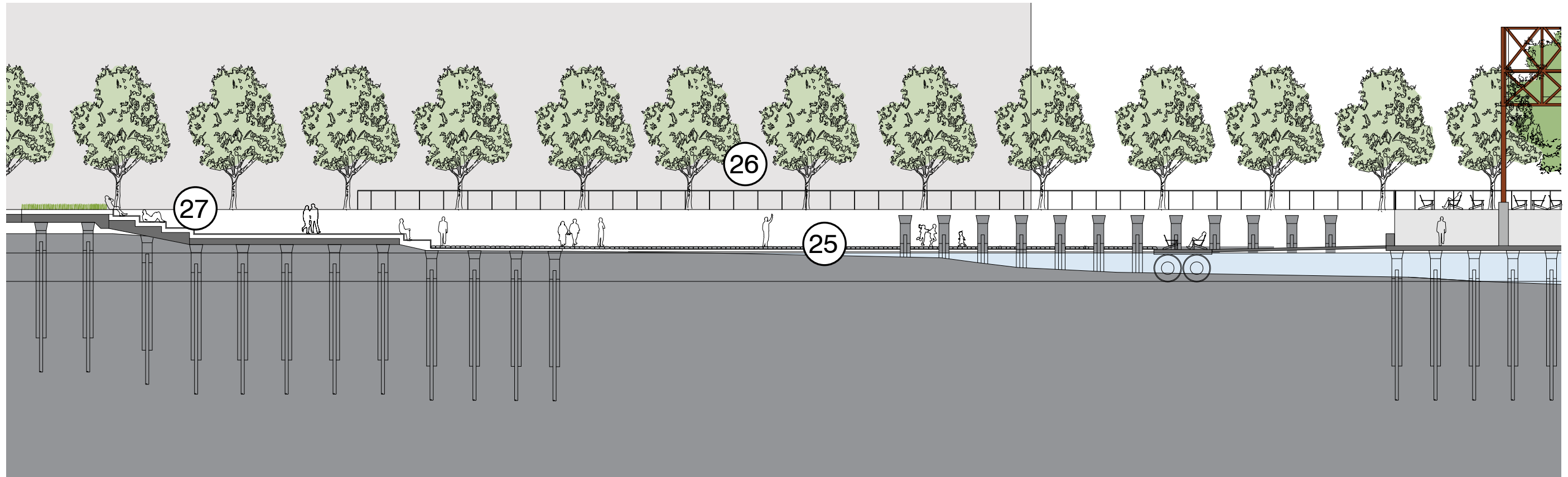
- 26. Upper Trail
- 27. Stone Terraces with wood ramp



SECTION E



SECTION West to East Through Center of Deck



0 20' 40'

SECTION F



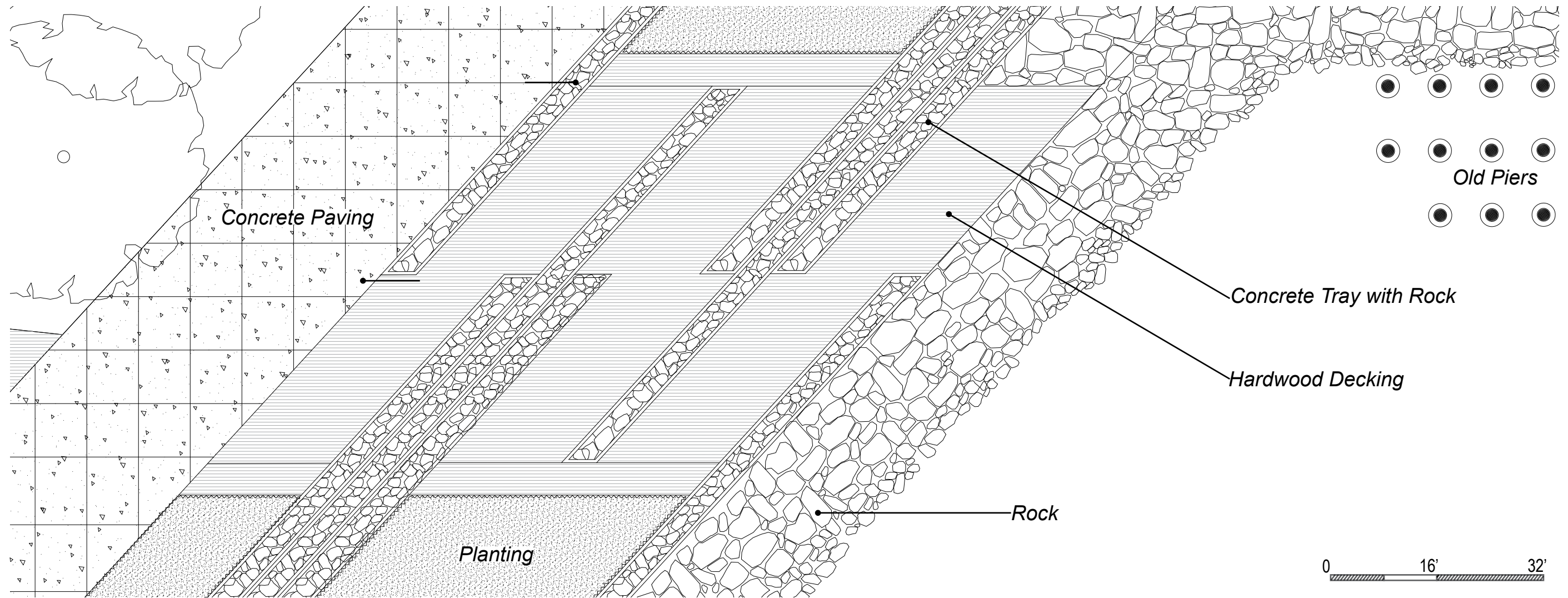
Wood and Stone

Rocks and timbers are combined to create trays of landscape with a sloping ramp for access to the north



Near Water Informal Trail

Rocks and gravel are used to make an informal walkable surface that approaches the Mean High Water. Remnant piers are left from old wharf structure.



DETAIL PLAN

Concrete Paving

Hardwod Decking

Planting

Rocks

Concrete Tray with Rocks

Old Piers





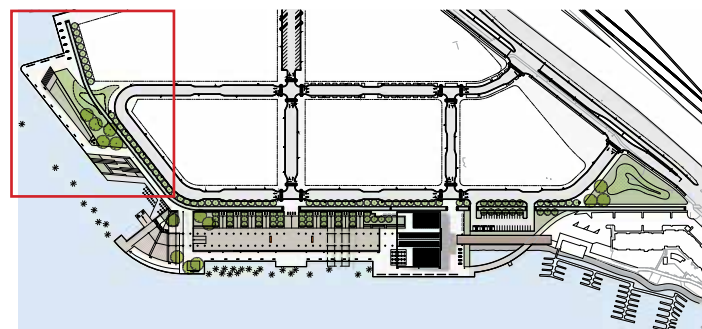
PERSPECTIVE VIEW FROM THE COVE LOOKING SOUTHEAST

Concept Image



The incline is both a belvedere with bigger water views from higher elevations and an urban hill for play or lounging.

Key Plan

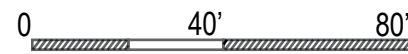


- 26. Upper Trail
- 27. Stone Terraces with Wood Ramp
- 28. Sloped Lawn Plane
- 29. Public Bathroom
- 30. Stormwater Treatment Garden

- 31. Stepped Seating
- 32. Picnic and BBQ Area



SECTION G



Tilted Lawn Plane

Lawn at 5% maximum slope will face southward for maximum solar gain. Slope will provide a place to play and a raked surface for watching movies or other events while also providing better water views and a sense of prospect.



Public Restrooms

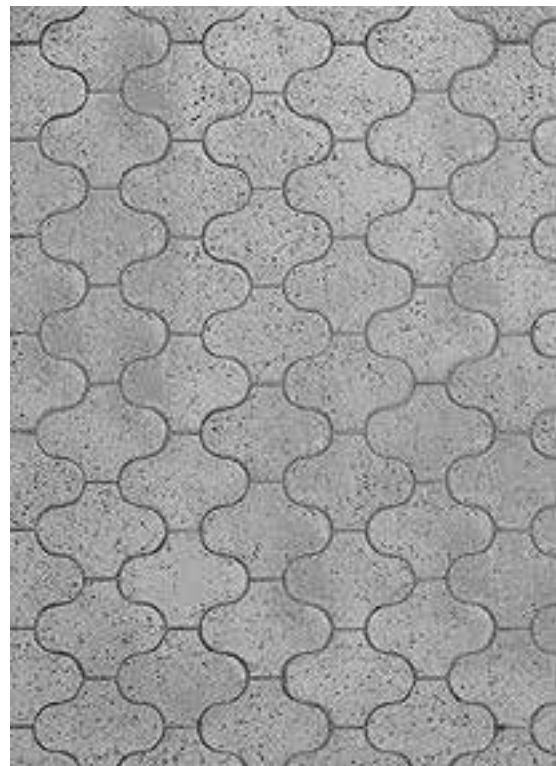
Public restrooms shall be simple and well designed. Stalls shall be individually accessed from the exterior with outdoor washing stations. Materials should be easy to clean and resilient.



PERSPECTIVE VIEW FROM STORMWATER TREATMENT GARDEN BIKE PATH WITH INCLINE BEYOND



Corten Steel



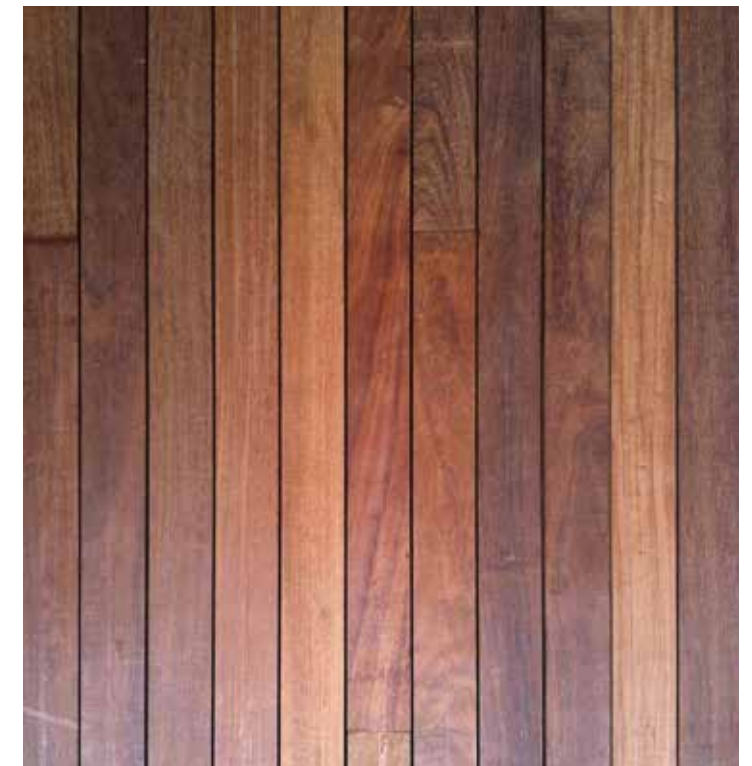
Stamped and Colored Concrete



Colored Concrete with Acid Etch



*Colored Concrete with Wood**

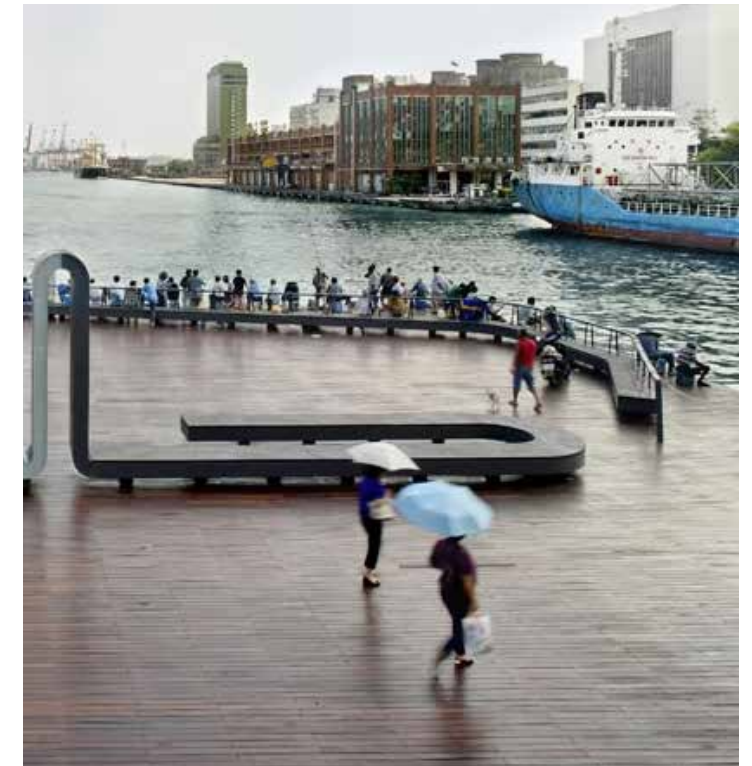


*Hardwood***



**Salvaged timbers from the 9th Avenue Terminal Roof will be re-used to the extent possible for site furniture*

***Salvaged timbers from the 9th Avenue Terminal Roof will be evaluated for use as the decking, but may likely be too soft and an FSC harvested deck board will be used if needed.*





Wood Boardwalk and Rock



Colored Concrete with Acid Etch



Rock and Gravel



Gravel with Gravel Pave 2 Rings



Wood Piles





Eleocharis sp. +
Spike Rush



Juncus balticus + <>
Baltic rush



Juncus patens + <>
Juncus



Juncus patens 'Elk Blue' + <>
Elk Blue Juncus



Scirpus microcarpus +
Bulrush



Festuca rubra 'Molate Blue' + <>
Molate Blue Fescue



Festuca rubra + <>
Red Fescue



Baccharis pilularis pilularis + <>
Coyote Bush



Grindelia stricta 'venulosa' + <>
Gum Plant



Calycanthus occidentalis +
Spice Bush



Myrica californica + <>
Pacific Wax Myrtle

+ Native
<> Low Water

Plant material species shown are representative of concept and design goals for planting.
Water Efficient Landscaping and the use of native and endemic plants is a design criteria for our selections.
Final selection of plants will be based on plant quality, availability, and season.
Additional changes may be made to achieve a unified design following any required changes.



Rhamnus (Frangula) Californica +<>
Coffeeberry



Olea 'Montra' <>
Dwarf Olive



Arctostaphylos sp. +<>
Manzanita



Fragraria chiloensis +
Creeping Strawberry



Gazania sp.
Gazania



Verbena bonariensis <>
Verbena



Lomandra Longifolia 'Breeze' <>
Lomandra

Bolero Dwarf Fescue Sod



Leymus triticoides 'Lagunita' + <>



Low Water Lawn



Sesleria autumnalis 'Greenlee' <>
Sesleria Greenlee



Miscanthus 'Little Kitten'
Miscanthus 'Little Kitten'

+ Native
<> Low Water

Plant material species shown are representative of concept and design goals for planting.
Water Efficient Landscaping and the use of native and endemic plants is a design criteria for our selections.
Final selection of plants will be based on plant quality, availability, and season.
Additional changes may be made to achieve a unified design following any required changes.



Cupressus macrocarpa + <>



Quercus species, natural form + <>



Platanus racemosa, single trunk + <>



Arbutus marina, multistem + <>



Rosa 'Cecile Brunner' <>



Purple Trumpet Vine



Plumbago <>

Plant material species shown are representative of concept and design goals for planting. Water Efficient Landscaping and the use of native and endemic plants is a design criteria for our selections. Final selection of plants will be based on plant quality, availability, and season. Additional changes may be made to achieve a unified design following any required changes.

+ Native
<> Low Water



Trash, Recycling, Compost (Landscape Forms)



Bike Rack, Inverted U (Creative Pipe)



Concrete Slab Bench



*Pole Mounted "Theatre Lights"
(Also mounted on trellis)
Installed with downward facing "full cut off" condition*



Movable Chairs (Loll)



Custom Large Table with Moveable Chairs



Custom Large Concrete Table with Benches



Cafe Tables and Chairs (Landscape Forms)

Site furniture selections describe character and quality of proposed elements. Final selections will be made within the same framework, but may vary from the pieces shown here.



Metal BBQ



Wood and Metal Picnic Table (ADA)



Slim Profile LED pole Light, Trails Only



Custom Large Timber Bench



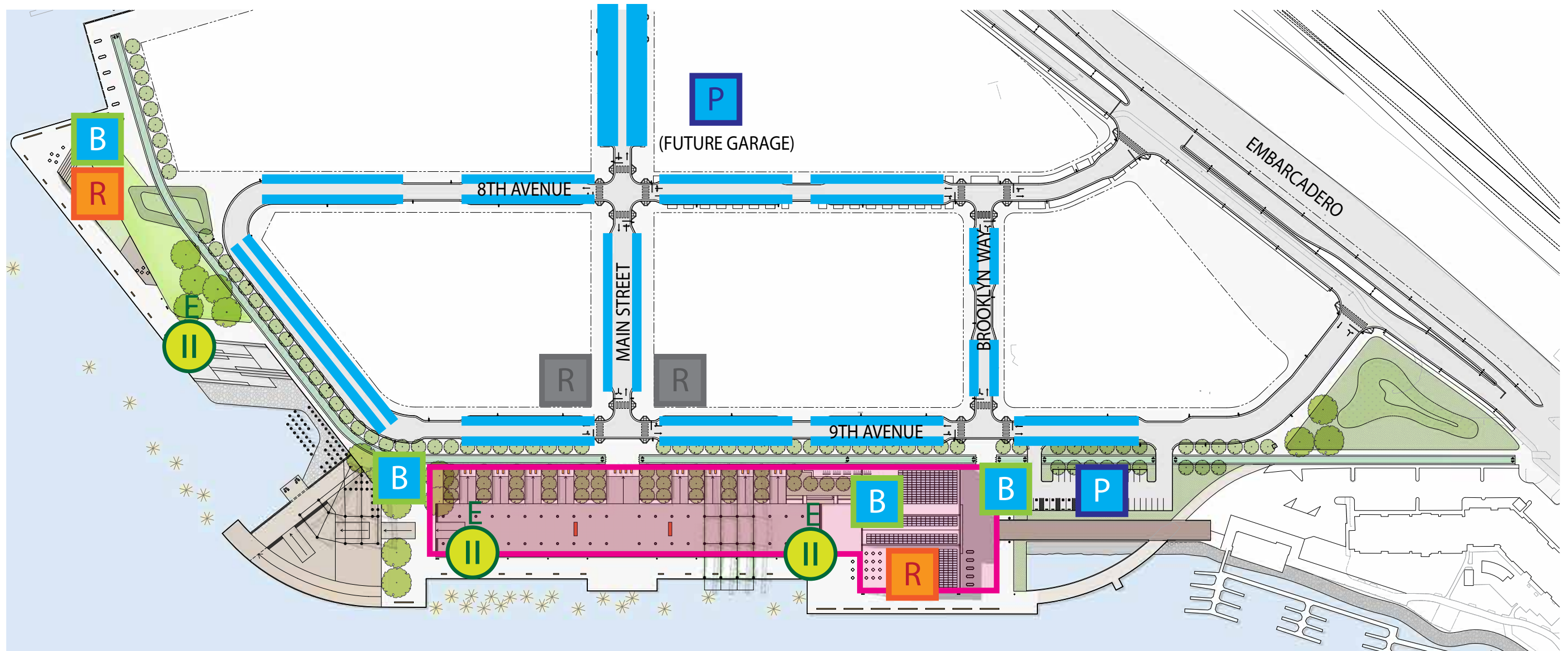
Lounge Bench

Site furniture selections describe character and quality of proposed elements. Final selections will be made within the same framework, but may vary from the pieces shown here.



All public art proposed at the site will use light as one design element. Priority will be given to interactive pieces that can be used both during the day and at night and allow children and adults chances for playing and wonder.

Public Art imagery above describes character and quality of proposed elements. Final selections will be made within the same framework, but will vary from the pieces shown here.



- Street Parking
- P Parking Lot or Garage
- R Public Restrooms
- E
|| Event Power
- B Bike Parking
- R Retail Restrooms
- Event Area

DESIGN APPROACH

Although the 9th Avenue Terminal is not designated a historic property, its essential landmark character will be maintained; the renovation will refer to its historic usage and highlight the warehouse's character-defining elements. Per the Secretary of the Interior's Standards for Rehabilitation, the 20,000 sf of the Terminal will be restored with a clear differentiation between new elements and repaired historic elements.

RETAINING THE HISTORIC ELEMENTS OF THE TERMINAL: EXTERIOR

The iconic front façade will be renovated and repainted, and the original cast concrete facades on the water and docksides will be repaired and restored. Careful consideration will be given to the overall color scheme as old and new elements are combined.

The terra cotta details on the lintels over the main doors and the corner elements will be restored, stripping the paint that has covered them from view. Some work has already been done on the south elevation to reveal the original color.

All the windows will be re-glazed to meet Title 24 energy requirements. Different ways of achieving energy goals will be evaluated to determine the best and most economical method; it is likely to include preserving and re-glazing the steel sash single-pane windows, adding weather stripping and glazing film or interior storm windows.

The new parkside entry wall that closes off the 20,000 sf of interior space will be contemporary, contrasting with the original cast concrete facades on the sides. The connection will be detailed to contrast and highlight the original shell of the exterior. The new wall will be cement plaster in finish, but glazed up to the bottom of the uppermost existing clerestory truss. There will be operable glazed walls for access to the deck.

The existing canopy over the former docks on the north side of the building will be replaced with a glass and steel canopy, and a similar new glass and steel canopy will be added at the front entry. It is important to note that this contemporary design is not meant to confuse what is historic and what is new; but rather to highlight the new stage in the building's history. The restored roof will show 12 skylights over the covered portion of the deck, with the ridgeline featuring two ventilators as in the original design.

The plan is to expose two bays of the existing steel structure to provide covered, but not enclosed, space over a deck. The open truss work will evoke the character of the space as warehouse. The only additions



to the steel trusses will be for structural stabilization. New sections will be added at the same height as the existing bracing elements, and will contrast with the existing truss in order to continue to differentiate between old and new.

The existing exposed columns have a concrete base, and we propose a similar treatment at each base. Populating the deck will be a moveable seating area and a portable kiosk.

RETAINING THE HISTORIC ELEMENTS OF THE TERMINAL: INTERIOR

We will retain the historic harbormaster's offices. The offices are proposed to be transformed into a café with access from the front of the entrance plaza as well as from the interior of the enclosed space.

The main interior 20,000 SF space is currently defined to include a water-side restaurant, and a maritime display. On both waterside and former dock locations, there will be operable walls that open up to the covered deck area.



FRONT OF BUILDING

Restore signage
Repair & paint cast concrete
Restore glazing



TERRACOTTA DETAIL ON SIDE FAÇADE

Restore tile ornamentation



FRONT CORNER - WATER SIDE

Restore tile ornamentation
Repair & paint cast concrete
Restore glazing



LAND SIDE

Repair & paint cast concrete
Restore glazing
Replace canopy



TERRA COTTA DETAIL ON FRONT FAÇADE

Restore terra cotta tile ornamentation
Repair & paint cast concrete



FRONT DOOR DETAIL

Restore terra cotta tile ornamentation
Repair & paint cast concrete

CANOPY



ENTRY



PROPOSED COLOR PALETTE



COLOR #1 - CAST CONCRETE ACCENT



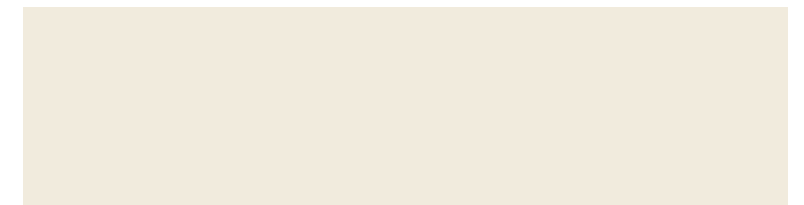
COLOR #2 - STEEL SASH WINDOWS



COLOR #3 - CAST CONCRETE



COLOR #4 - STOREFRONT ENTRIES & STEEL CANOPIES

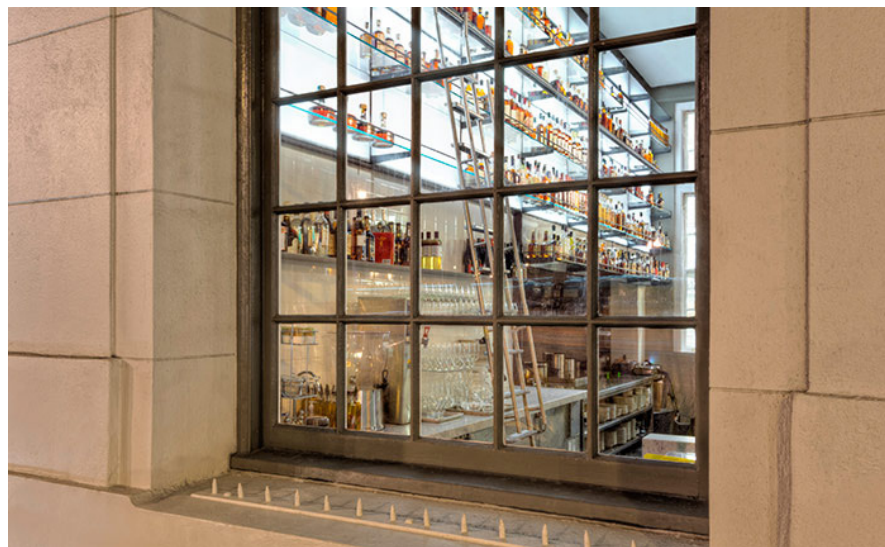


COLOR #5 - STEEL TRUSSES & COLUMNS



COLOR #6 - CEMENT PLASTER WALL

WINDOW

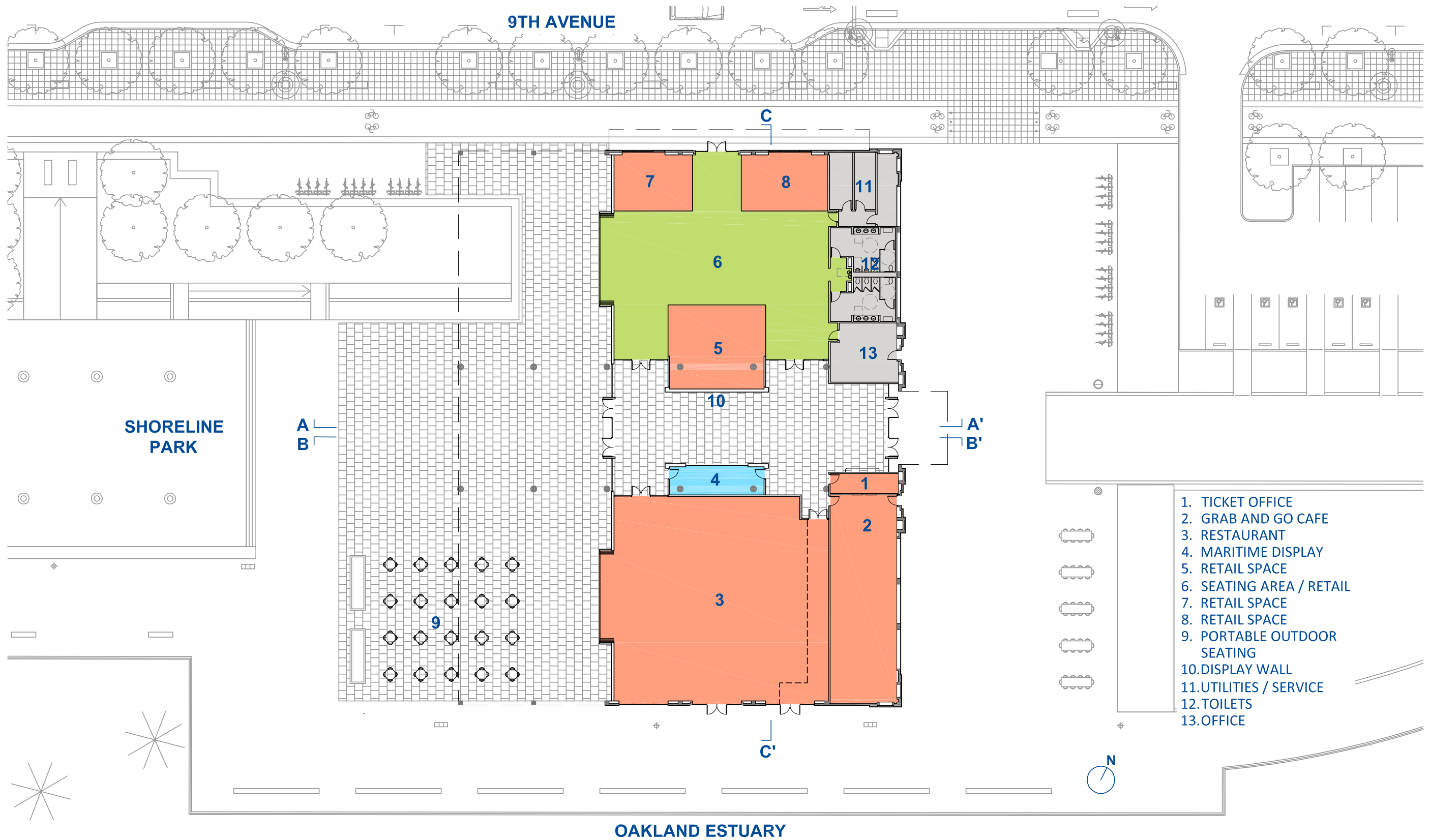


INTERIOR



BINDING AREA

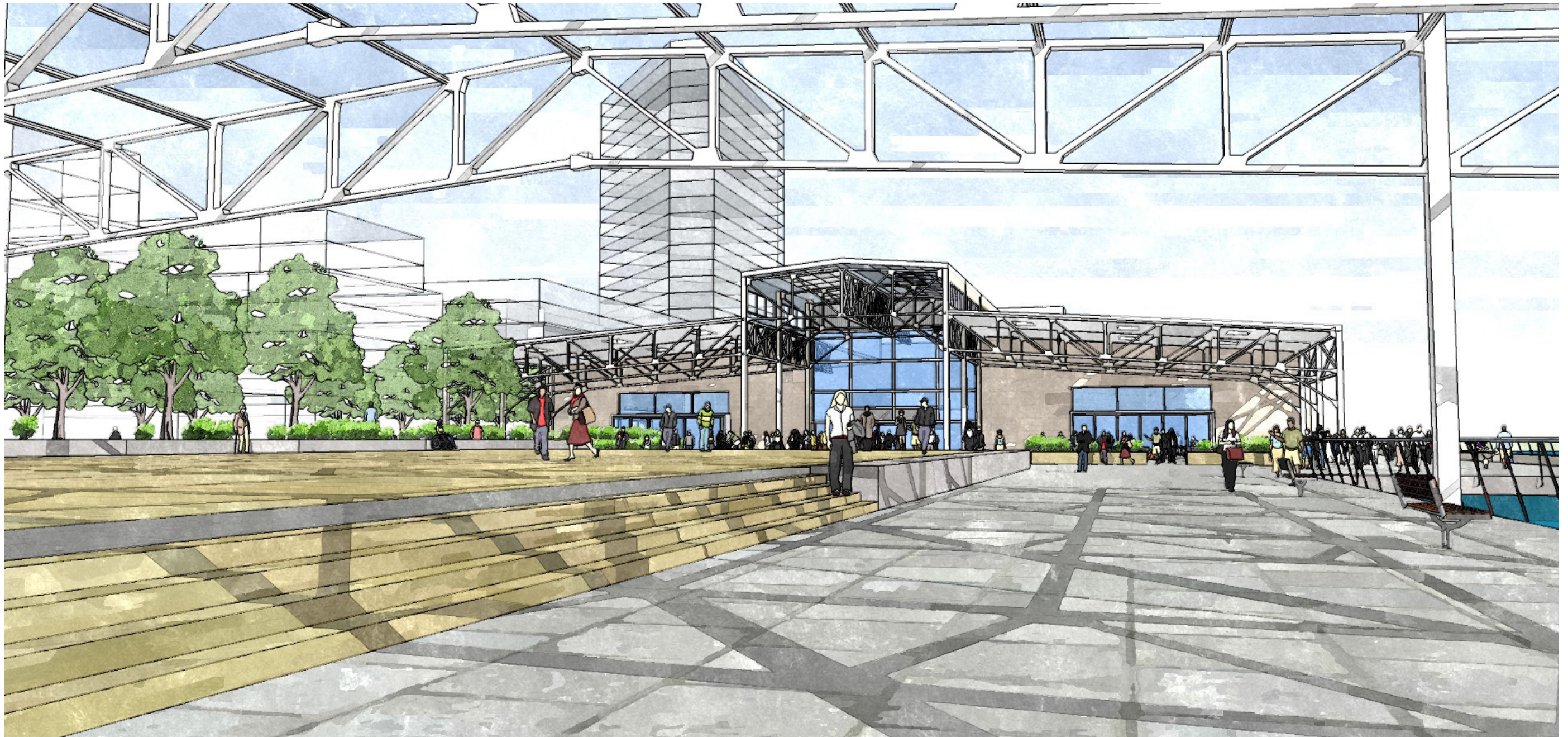
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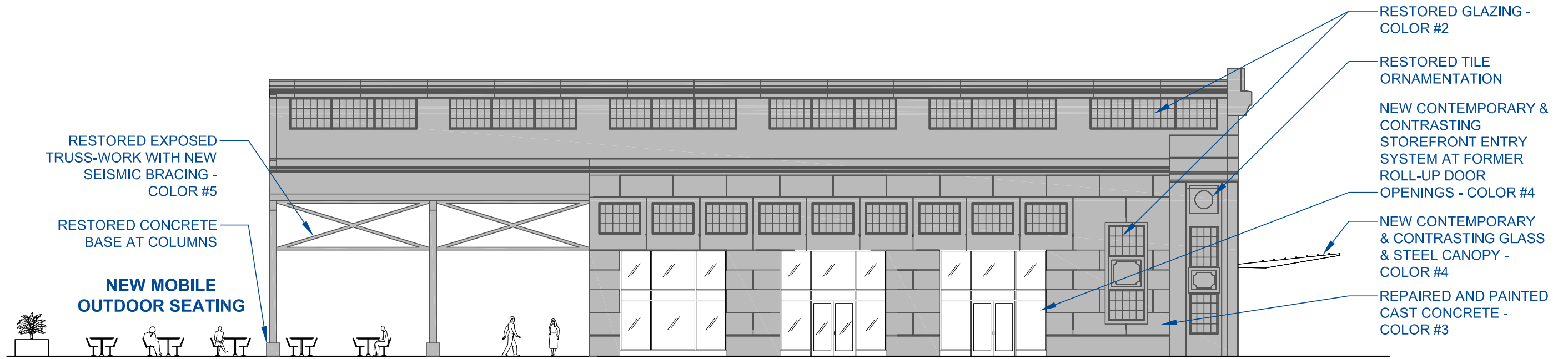


BINDING AREA

BINDING AREA

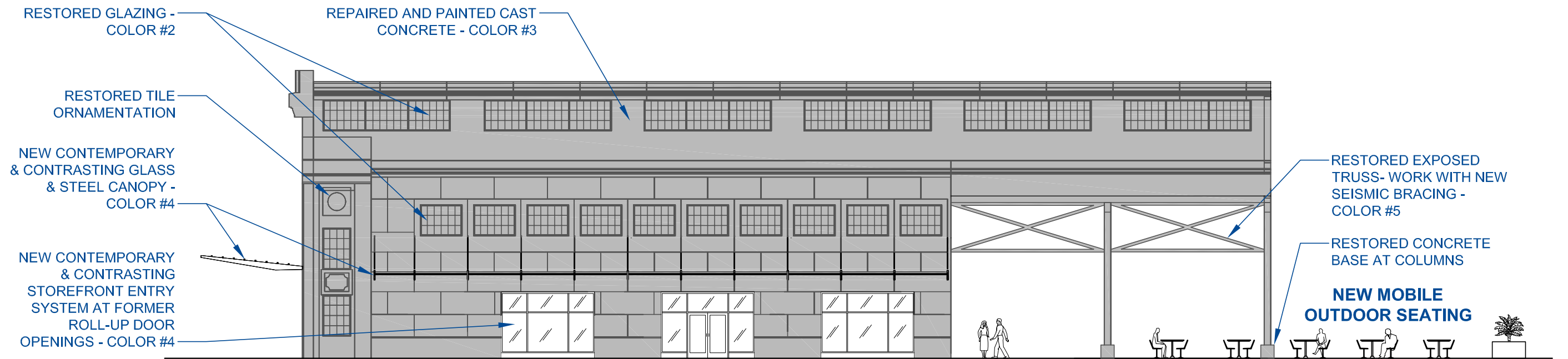


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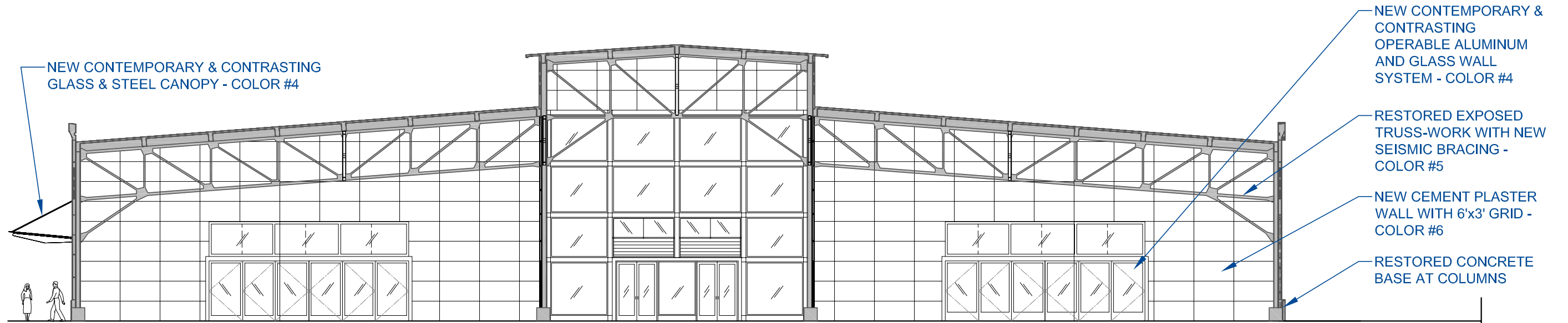
SOUTH ELEVATION - WATER SIDE

BINDING AREA



NORTH ELEVATION - LAND SIDE

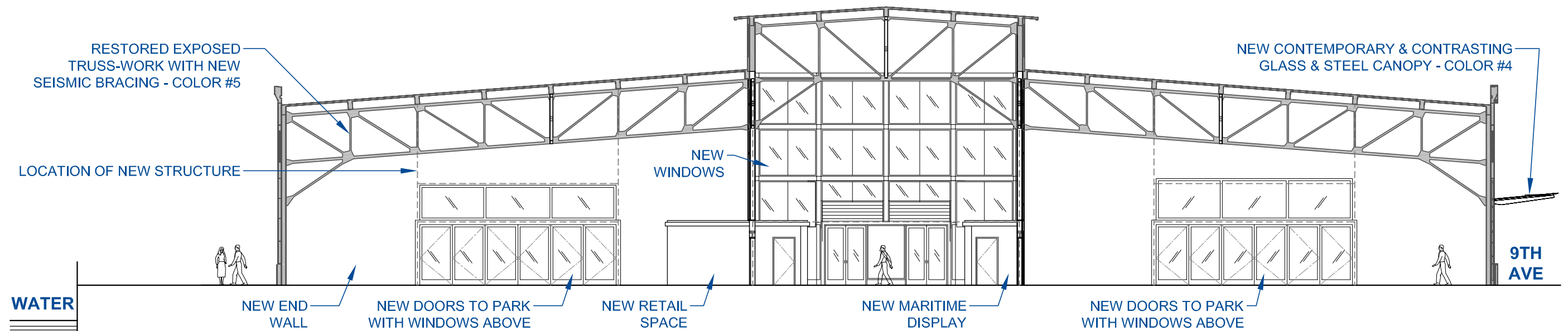
BINDING AREA



WEST ELEVATION - PARK SIDE

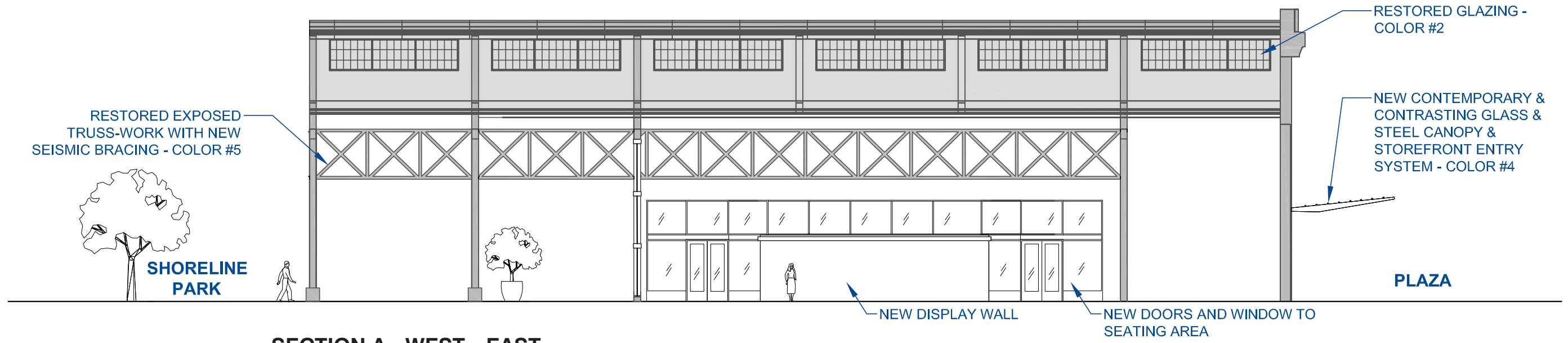
WATER

BINDING AREA



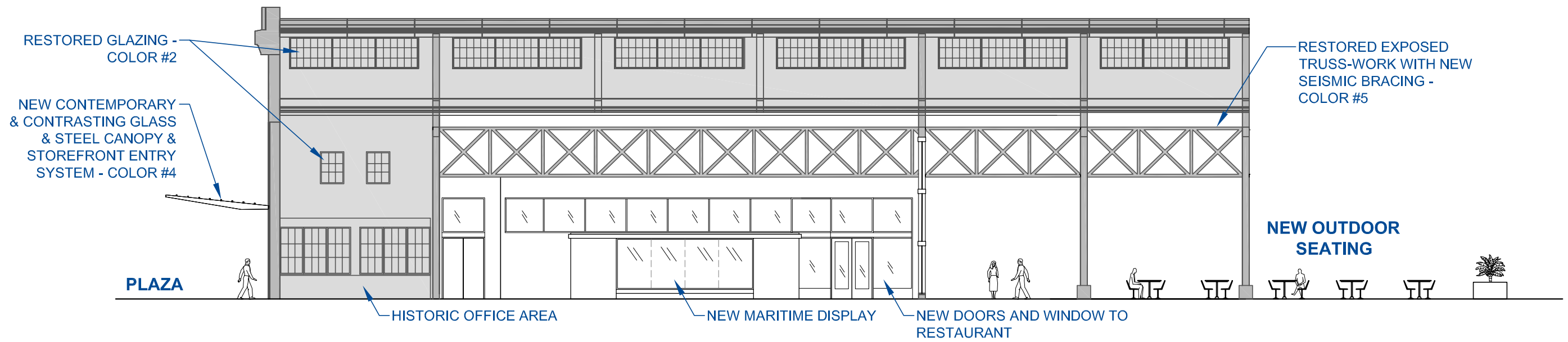
SECTION C - SOUTH—NORTH

BINDING AREA



SECTION A - WEST—EAST

BINDING AREA



SECTION B - EAST—WEST

BINDING AREA

BINDING AREA

